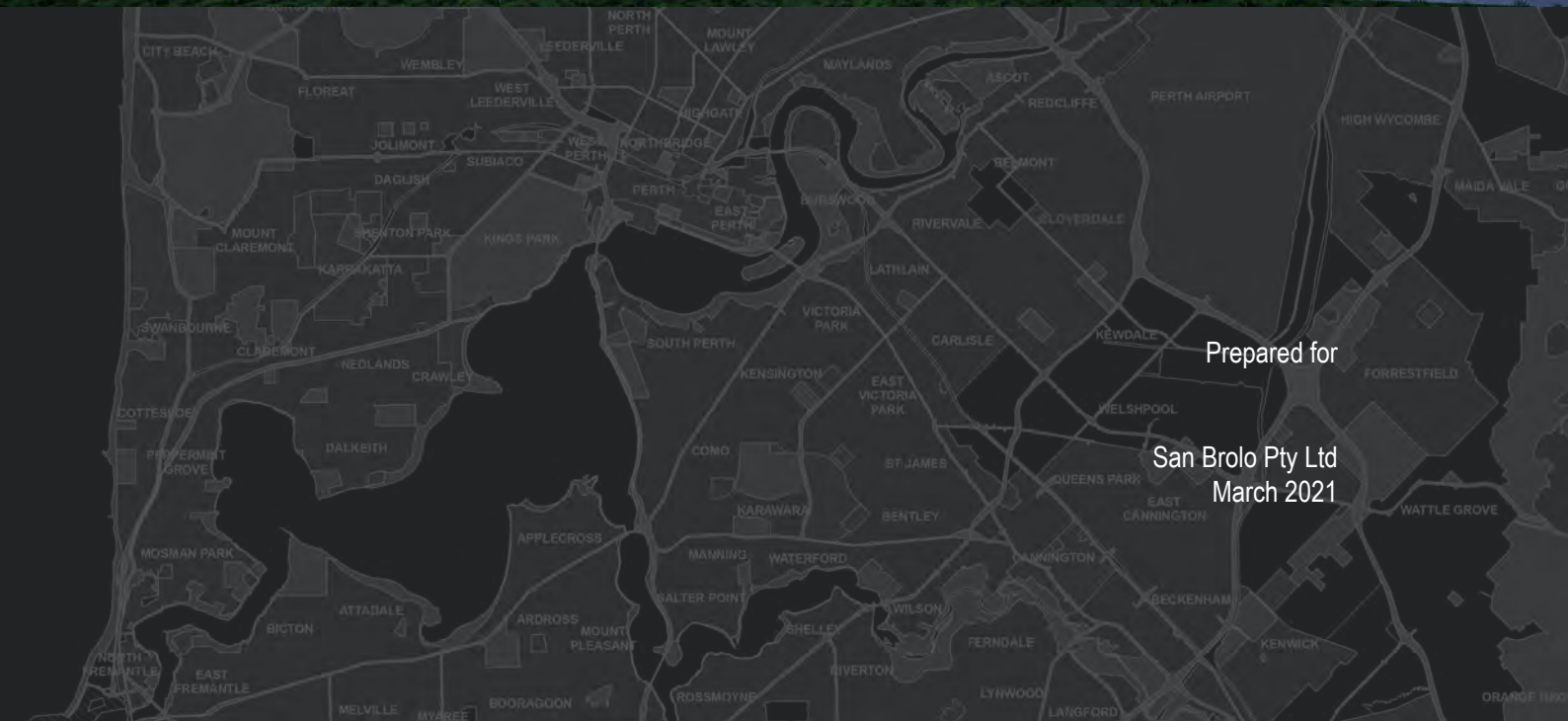


Development Application Proposed Multiple Dwelling Development

Strata Lot 1 (25) The Esplanade and Strata Lot 2 (20) Helm Street
Mount Pleasant, WA

PLANNING SOLUTIONS
URBAN & REGIONAL PLANNING

PS



Prepared for

San Brolo Pty Ltd
March 2021

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Contents

1	Preliminary.....	3
1.1	Introduction	3
1.2	Background	3
1.2.1	Pre-lodgement consultation	3
2	Site Details.....	5
2.1	Land Description	5
2.2	Location & Context.....	5
2.2.1	Regional context	5
2.2.2	Local context.....	5
2.3	Land Use and Topography.....	6
3	Proposed Development.....	12
3.1	Proposed Development.....	12
3.2	Access and Parking	12
3.3	Landscaping.....	13
3.4	Waste Management.....	13
3.5	Design.....	13
4	Strategic planning framework.....	17
4.1	Directions 2031 and Beyond	17
4.2	Central Metropolitan Perth Sub-regional Strategy.....	17
5	Statutory planning framework	18
5.1	Metropolitan Region Scheme	18
5.2	City of Melville Local Planning Scheme No. 6.....	18
5.2.1	Scheme Amendment No. 9	18
5.3	Canning Bridge Activity Centre Plan	19
5.4	State Planning Policies	21
5.4.1	State Planning Policy No. 7 – Design of the Built Environment.....	21
5.4.2	State Planning Policy 2.10	21
5.4.3	State Planning Policy No. 4.2 Activity Centres for Perth and Peel	21
5.5	Local Planning Policies	22
5.5.1	LPP1.20 – Canning Bridge Activity Centre Plan Density and Bonus Provisions	22
6	Development Assessment	23
6.1	Canning Bridge Activity Centre Plan – Design Guideline Assessment	23
6.2	State Planning Policy 7.3 – R-Codes Vol 2 Apartments.....	29
6.2.1	State Planning Policy 7.3 – R-Codes Volume 2 Element Objectives	29
7	Performance Assessment.....	32
7.1	Dwelling diversity	32
7.2	Street setbacks	33
7.3	Side/rear setbacks	34
7.4	Car Parking	35
7.5	Site Levels.....	36
8	Conclusion.....	37

Appendices

Appendix 1:	Certificate of Title
Appendix 2:	Site Survey
Appendix 3:	City of Melville Design Review Panel Minutes (03.03.21)
Appendix 4:	Development Plans
Appendix 5:	Design Report (SPP7 principles assessment)
Appendix 6:	Transport Impact Statement
Appendix 7:	Landscaping Plans
Appendix 8:	Waste Management Plan
Appendix 9:	Sustainability Report
Appendix 10:	Geotechnical Report

1 Preliminary

1.1 Introduction

Planning Solutions acts on behalf of San Brolo Pty Ltd, the proponent of the proposed development at Strata Lot 1 (20) Helm Street and Strata Lot 2 (25) The Esplanade, Mount Pleasant (**subject site**). Planning Solutions has prepared the following report in support of an Application for Development Approval for a four-storey multiple dwelling development at the subject site.

This report will discuss various issues pertinent to the proposal, including:

- Background.
- Site details.
- Proposed development.
- Planning considerations.

The proposal involves the development of 12 apartments in a four-storey building form, along with associated basement parking and landscaping on a 1,287m² site. The proposal will provide the locality with appropriately located and scaled apartments, maximising the efficient and sustainable use of land adjacent public transport hubs through the provision of a high-quality, medium density development which adds to the vibrancy and amenity of the local area.

1.2 Background

1.2.1 Pre-lodgement consultation

Prior to lodging the application for development approval with the City of Melville, the proponent engaged with the City of Melville (**City**) to inform the design and confirm key issues to be addressed. A summary of this pre-lodgement consultation is provided in **Table 1** below.

Table 1: Pre-lodgement consultation summary

Stakeholder	Date consulted	Method	Key outcomes
City of Melville (Planning)	19 February 2021	Meeting	<p>The proponent, architect and Planning Solutions met with representatives of the City of Melville to discuss the initial development concept and likely development application assessment process. The City advised that the development concept can be considered at an initial Design Review Panel meeting review prior to lodgement.</p> <p>The City provided feedback on early versions of the concept plans. The consultant reporting to accompany the development application was also discussed and agreed in principle.</p>
City of Melville Design Review Panel	3 March 2021	Design Review Panel Meeting	<p>The project team presented the design concept to the City's DRP. Six of the 10 design elements were 'supported' with the remaining four subject to further information. A copy of the DRP minutes are provided at Appendix 3, along with an application response to the comments. A brief summary of the DRP minutes are as follows:</p> <ul style="list-style-type: none"> - The Panel commended the applicant's efforts thus far and looks forward to seeing how the façade materiality, texture and colour develop in a way that references the local context and character of the area. - Encouraged applicant to provide streetscape elevations and introduce gated entry to the ground floor units from the public domain.

			<ul style="list-style-type: none">- Recommended relocating the roof terrace forward to have a more strategic relationship with the view and vista.- Recommended removing the drop off car bay and provide additional landscaping in its place.- Recommended further consideration of visual screening devices where they protrude into setbacks.- Need to consider/address balcony sizes for dual key apartments to ensure access to natural light, view and vista.- Encouraged to consider a more generous light well to the individual lift lobbies as well as a more generous and legible lift lobby at the ground floor.
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2 Site Details

2.1 Land Description

Refer to **Table 2** below for a description of the subject site.

Table 2: Lot details

Strata Lot	Plan / Diagram	Volume	Folio	Area (m ²)
1	Strata Plan 7764	1565	464	1,287
2	Strata Plan 7764	1565	465	

Refer **Appendix 1** for a copy of the Certificate of Title and associated Strata Plan.

The subject site is not subject to any limitations, interests, encumbrances and/or notifications materially relevant to the proposed development.

2.2 Location & Context

2.2.1 Regional context

The subject site is situated within the local government municipality of the City of Melville and is located approximately 6.7 kilometres south of the Perth Central Business District, within the suburb of Mount Pleasant. The subject site fronts The Esplanade and Helm Street.

The subject site is highly accessible, located in close proximity to Canning Highway, a major arterial road that provides access to the wider Perth metropolitan region, located 300m from the site. The subject site also is located approximately 650m west of the Canning Highway access ramps to the Kwinana Freeway northbound and 890m from the Manning Road access ramps to the Kwinana Freeway southbound.

The subject site is highly accessible by public transport from Canning Highway which has regular bus services running every 2-5 minutes during peak hours (7am – 9am), approximately 290m from the site. These bus routes include services 111, 114, 115, 160 and 910. Canning Bridge Train Station is situated approximately 850m east of the subject site, with train services running between Perth City and Mandurah.

The site is therefore conveniently located for medium density residential development, taking advantage of the proximity to this infrastructure and services.

2.2.2 Local context

The subject site is surrounded by a mix of low, medium and high density residential and commercial uses.

To the south and west of the subject site is primarily two – three storey residential development. To the north of the subject site, the residential density and commercial development increases as The Esplanade gets closer to Canning Highway.

The subject site is an area of transition with new residential and commercial development of varying intensities being constructed. This includes three completed high-rise residential developments (20 – 33 storeys) within 300m of the subject site, shown in **Figure 1**.

In terms of immediate local context, the subject site is surrounded by:

- To the north on the opposite side of Helm Street, two-storey grouped residential developments.

- To the east on the opposite side of The Esplanade, the Swan Canning River Parks and Recreation reserve.
- To the south, adjacent to the subject site, a three storey single dwelling.
- To the west, adjacent to the subject site, a three storey single dwelling.

Refer **Figure 1** for an Aerial Photograph illustrating the subject site's location and context.

2.3 Land Use and Topography

The subject site currently contains two grouped dwellings, both two storeys in height. Both dwellings are to be demolished as part of this proposal.

Vehicle access to the subject site is from Helm Street via a combined crossover approximately 9m wide.

The subject site has a gradual slope downwards from the north west corner to the eastern boundary, where there is approximately a 1m level difference.

Refer **Appendix 2** for a copy of the Site Survey.

Refer **Photographs 1 – 6** below, illustrating the streetscape and surrounding land uses.



Photograph 1: View along Helm Street, looking west from The Esplanade.



Photograph 2: View of residential dwellings opposite side of Helm Street looking north west.



Photograph 3: View along The Esplanade, looking south west.



Photograph 4: View of Helm Street facing east with the subject site on the right hand side.



Photograph 5: View of development along The Esplanade, looking north.



Photograph 6: View of commercial and high-rise residential development on The Esplanade, looking north.



Photograph 7: Subject site as viewed from The Esplanade at the corner of Helm Street



Photograph 8: Subject site as viewed from the opposite side of The Esplanade



Photograph 9: Subject site as viewed from Helm Street, facing south-east



LEGEND

- Subject Site
- 1 Sabina
- 2 The Precinct
- 3 Cirque

3 Proposed Development

3.1 Proposed Development

The proposal involves a four storey development comprising 12 apartments. Car parking and other building services are located on the basement level, with the apartments, communal areas and landscaping located on the ground to third floors, and the roof terrace. The particulars of the proposed development are outlined in **Table 3** below.

Table 3: Development Particulars

Level	Development Particulars
Basement	<p>The basement level contains the following:</p> <ul style="list-style-type: none"> • Parking – 20 resident bays and 2 communal electric car charging bays. All bays accessed via the single lane ramp with crossover from Helm Street. Pause bay provided to ensure safe traffic flows. • 25 x bicycle bays • Water tank • 2 x services room • 1 x bulky storeroom • 1 x general storeroom • 14 x residential storerooms (varying sizes between 4m² and 23m²)
Ground Floor	<ul style="list-style-type: none"> • Entrance and lift lobby (50m² internal area) • 1 x two-bedroom apartment • 2 x three-bedroom apartments (dual key configuration) • 4 x visitor bicycle bays • 152.5m² deep soil planting areas (11.85% of site).
First Floor	<ul style="list-style-type: none"> • 2 x three-bedroom apartments (dual key configuration) • 1 x three-bedroom apartment • 1 x bin room (~3.5m² area) • 12m² on structure landscaped garden area • 1 x service area (~3m² area)
Second and Third Floor	<p>Each floor contains:</p> <ul style="list-style-type: none"> • 2 x three-bedroom apartments (dual key configuration) • 1 x three-bedroom apartment • 1 x bin room • 12m² on structure landscaped garden area • 1 x service area
Roof	<ul style="list-style-type: none"> • Communal garden (174m²) • Screened enclosed air conditioning units • Solar photovoltaic cells

3.2 Access and Parking

Vehicle access is proposed via a single crossover/access point from Helm Street. The proposed access point provides a one way ramp into the basement car park and is accessible for residents only. It is proposed to coordinate traffic along the access ramp using a traffic light system and two holding bays at each end of the ramp.

Proposed onsite parking includes 22 bays on the basement level. 20 bays will be allocated to residents and two bays will be for communal electric vehicle charging.

The subject site will have a trip generation of approximately 11 trips during the AM peak hour period and approximately 12 trips in the PM peak period. These volumes are very low and can be accommodated within the existing capacity of the local road network (refer **Appendix 6**, for a copy of the Traffic Impact Statement).

3.3 Landscaping

The proposed development provides landscaping at ground and upper levels, comprising an attractive mix of plant species of small to large scale. The proposed landscaping will enhance the overall presentation of the development and improve the visual appearance of the development and streetscape, as well as enhancing the amenity of the communal spaces.

The deep soil area makes up 152.50m² of the subject site (11.85%) and provides an attractive view of the building from the street for residents and visitors to enjoy. Landscaping has been carefully located throughout the site, including:

- The green wall provided as an entry feature in the lobby space.
- Feature trees on the ground floor entry, roof terrace, and south eastern corner of the development to soften the harder areas of the design and to provide feature views.
- The ‘floating’ entry accessed via a boardwalk over water with feature pots and stone paving.
- The entry water feature with integrated artwork.

The communal garden located on the roof terrace of the proposed development provides 174m² of communal space for residents to enjoy. The rooftop garden area provides a deciduous feature tree, along with fruit trees and herbs for communal use, lush screening vegetation to the southern adjoining property, and outdoor dining / BBQ areas.

Refer **Appendix 7**, for a copy of the Landscaping Plan.

3.4 Waste Management

Dallywater Consulting have prepared a Waste Management Plan to support the proposed application. Waste from the development will be stored in ventilated bin stores on each of the apartment levels. Bins will be collected from the verge along Helm Street. Being a corner site with a large street frontage, there is sufficient space in the verge area to accommodate all bins. As per the Waste Management Plan, the complex/strata manager will oversee the relevant aspects of waste management.

Refer to **Appendix 8** for a copy of the Waste Management Plan.

3.5 Design

The design of the proposed development carefully considers and complements the existing context of the site, whilst balancing the need for residential infill, with the design providing spacious, adaptable apartments and appropriate high-quality landscaping.

Dwelling types, sizes and configurations have been designed to accommodate a diverse range of households and changing dwelling configurations. This allows for multi-generational housing which can accommodate live-in carers for aging in place. Internal and external living areas are generous, allowing for flexibility in accommodation and functionality.

The form of the building responds to the localised context, maximising cross ventilation and solar access whilst protecting visual privacy to adjoining properties. The design responds to the prevailing character of the immediate locality, maximising Swan River view access for all apartments, contributing to residents’ amenity.

A number of passive and active design strategies as well as renewable technology has been incorporated into the design to achieve a high energy efficiency rating for the development. These initiatives include high ceilings, high performance double glazing, secure bicycle parking for residents and visitors, cross ventilation for each apartment, communal electric vehicle (EV) charging bays, photo-voltaic solar panels, LED lighting, heat recovery air-conditioning systems.

Refer to **Appendix 5**, for a copy of the Architectural Design Report and **Appendix 9** for the Green Star assessment.



Illustration 1: Proposed development as viewed from The Esplanade, looking south west



Illustration 2: Proposed development as viewed from, The Esplanade, looking south west



Illustration 3: Proposed development as viewed from, The Esplanade, looking north west



Illustration 4: Proposed development as viewed from, The Esplanade, looking west



Illustration 5: Proposed development as viewed from, Helm Street, looking south east



Illustration 6: View of the entry to the proposed development as viewed from, Helm Street, looking south

4 Strategic planning framework

4.1 Directions 2031 and Beyond

Directions 2031 and Beyond (**Directions 2031**) is the high-level strategic planning framework for the Perth and Peel region. The Directions 2031 framework proposes five strategic themes for a liveable, prosperous, accessible, sustainable and responsible city. The framework sets out a hierarchy of activity centres across the metropolitan region to equitably distribute services, amenities and employment opportunities. Directions 2031 also sets a target for 47 percent of new residential development to be urban infill.

The proposed development is consistent with the strategic objectives of Directions 2031 insofar as it promotes higher density infill residential development in an inner urban area.

4.2 Central Metropolitan Perth Sub-regional Strategy

The Central Metropolitan Perth Sub-Regional Strategy (**Sub-Regional Strategy**) provides more in-depth strategic planning for the growth of the Central Metropolitan Perth Region to deliver the outcomes sought by Directions 2031.

Under the Sub-Regional Strategy, the City of Melville (**City**) is required to increase its existing housing stock of 40,110 dwellings to achieve a target of an additional 18,480 dwellings by 2050. Canning Bridge is identified in the Sub-Regional Strategy as a district centre in line with State Planning Policy 4.2 – Activity Centres for Perth and Peel (**SPP4.2**), with a density target of 30 dwellings per hectare or greater. Accordingly, the proposed higher density, four storey, 12 multiple dwelling development is in line with the strategic vision of the Sub-Regional Strategy.

5 Statutory planning framework

5.1 Metropolitan Region Scheme

The subject site is zoned Urban under the provisions of the Metropolitan Region Scheme (**MRS**). There are no MRS reserves affecting the subject site.

The proposed development is consistent with the provisions of the MRS and can be approved accordingly.

5.2 City of Melville Local Planning Scheme No. 6

The subject site is zoned Residential with an applicable density code of R-AC0 under the provisions of the City of Melville Local Planning Scheme No. 6 (**LPS6**).

The objectives of the Residential zone are as follows:

- *To provide for a range of housing and a choice of residential densities to meet the needs of the community.*
- *To facilitate and encourage high quality design, built form and streetscapes throughout residential areas.*
- *To provide for a range of non-residential uses, which are compatible with and complementary to residential development to promote sustainable residential development.*
- *To maintain the compatibility with the general streetscape, for all new buildings in terms of scale, height, style, materials, street alignment and design of facades.*

The proposal seeks approval for a four storey development comprising a mixture of apartment sizes, including dual-key apartments incorporating a multi-generational design which aims to promote housing diversity and aging in place. The proposed development is of high quality design and is considered to have a positive contribution on the streetscape. The use of varying materials and articulation of the each of the elevations ensures the building has a positive interaction with the streetscape and the surrounding residential development as well as the adjacent Swan River. The proposed development is therefore consistent with the objectives of the Residential zone.

Land Use

The proposed use is classed as ‘Multiple Dwelling’. LPS6 does not list permissibility for residential land uses but it is considered that residential use is consistent with the objectives of the Residential zone and is therefore considered appropriate within the Residential zone.

5.2.1 Scheme Amendment No. 9

Amendment 9 to the City of Melville Local Planning Scheme No. 6 (**Amendment 9**) was initiated by Council on 17 November 2020. Amendment 9 (as initiated) seeks to modify the southern boundary of the CBACP so that the boundaries of the activity centre are modified to exclude the properties south of Helm Street, and to recode these properties from RAC-0 to Residential R20.

At the time of preparing this report, the proposed amendment is being advertised for public comment. Although Amendment 9 may require consideration against Clause 67 of the Deemed Provisions of the *Planning and Development (Local Planning Schemes) Regulations 2015*, little weight should be applied in the consideration of this development application due to inconsistencies with strategic planning documents. Specifically, Amendment 9 is inconsistent with the City’s Local Planning Strategy and is inconsistent with the objectives of the Canning Bridge planning framework which seeks to intensify development in close proximity to the train station and high frequency bus routes.

The City’s Local Planning Strategy (**LPS**) sets out boundaries for the Canning Bridge activity centre. The subject site is within these boundaries. The proposed amendment would remove the site and land subject to Amendment

9 from the activity centre area. By altering the boundaries of the activity centre and modifying this to a low density residential zoning, Amendment 9 is creating an inconsistency with the LPS.

Furthermore, the objectives of the CBACP are to increase the density and diversity of housing in and around the CBACP area, as well as providing sufficient development intensity to support high frequency public transport. Amendment 9 will lower the density of the CBACP area and is therefore inconsistent with the CBACP.

Lastly, Amendment 9 has been initiated against officer advice and is inconsistent with direction from the WA Planning Commission which recommended a comprehensive review of the Activity Centre Plan. In lieu of this, Amendment 9 seeks an ad hoc change.

The State Administrative Tribunal has considered the weight to be applied to draft planning schemes/amendments on multiple occasions (for instance *McDonald's Australia Limited and Presiding Member of the Metropolitan Central JDAP [2015] WASAT 146*). The SAT has found that the weight to be applied to a draft instrument is centred around whether an amendment is certain and/or imminent. For the reasons outlined above and noting the inconsistencies with strategic planning documents, Amendment 9 cannot be considered certain by any means. It is also far from being imminent, noting the public comment period is ongoing. Amendment 9 should therefore not be given little to no weight in assessment of this application.

5.3 Canning Bridge Activity Centre Plan

The Canning Bridge Activity Centre Plan (**CBACP**) has been prepared to guide development within the CBACP area and comprises land in the City of Melville and the City of South Perth, within a convenient walkable distance from the Canning Bridge bus and rail interchange. The CBACP establishes planning framework and design guidelines for land use, built form, development and infrastructure within the area.

Under the provisions of the CBACP, the subject site is situated within the Residential **H4** precinct, with a permissible height of up to 4 storeys and 16 metres.

The subject site is located within Quarter 2 – Ogilvie Quarter (**Q2**), which is envisioned to continue to attract strong growth in commercial and residential development. As the 'business' quarter of the CBACP, Q2 seeks to attract businesses to take advantage of the natural amenity of the quarter and the surrounding residential population for their workforce with offices and small scale hospitality development encouraged. The subject site is located within the outer portion of Q2 which is identified to remain residential only, to provide a buffer to lower scale residential development outside of the CBACP. The CBACP area has excellent connectivity to the Perth city centre and Curtin University, as well as Fremantle to the west, will attract businesses seeking to take advantage of the natural amenity of the quarter and the surrounding residential population for their workforce.

Refer to **Figure 2** zoning map below.

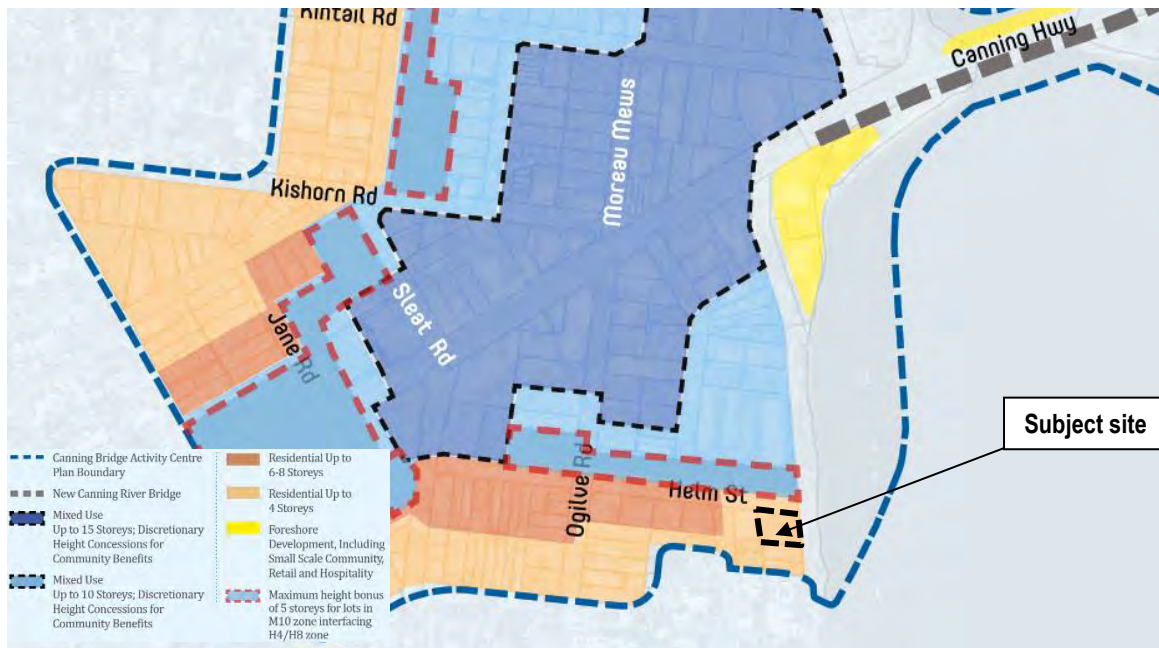


Figure 2: CBACP zoning map.

A detailed assessment table is provided in **section 6.1** of this report demonstrating the compliance of the proposed development with CBACP requirements. An assessment of the proposal against the objectives of the CBACP is provided within **Table 4** below:

Table 4: Assessment against the CBACP objectives

Objectives	Response
Meet district levels of community need and enable employment, goods and services to be accessed efficiently and equitably by the community.	The proposed development provides a mixture of apartment sizes, including dual-key apartments incorporating a multi-generational design which aims to promote housing diversity and aging in place. No commercial uses are proposed as part of this development. The proposal will provide a population base to support the goods and services to be provided within the Canning Bridge area.
Support the activity centre hierarchy as part of a long-term and integrated approach to the development of economic and social infrastructure.	The proposed development is consistent with the H4 development requirements and will provide suitable residential development to support the Canning Bridge activity centre.
Support a wide range of retail and commercial premises and promote a competitive retail and commercial market.	The proposed development is in the residential portion of the structure plan area. No commercial and retail uses are proposed as part of this development.
Increase the range of employment within the CBACP area and contribute to the achievement of sub-regional employment self-sufficiency targets	The proposed development is in the residential portion of the structure plan area. No commercial and retail uses are proposed as part of this development.
Increase the density and diversity of housing in and around the CBACP to improve land efficiency, housing variety and affordability and support the facilities in the area.	The proposed development incorporates 12 apartments providing diversity in housing options within the area. The development includes apartments of various sizes to cater for a diverse population, including some apartments with a dual-key design to provide further diversity in housing choice.
Ensure the CBACP area provides sufficient development intensity and land use mix to support and increase high frequency public transport.	The four storey, 12 multiple dwelling development is consistent with the design guidelines for the H4 area and will provide a population base to support the high frequency public transport within the locality.

Maximise access to and through the CBACP area by walking, cycling and public transport while reducing private car trips.	The proposed development is easily accessed by pedestrians and people with bicycles. The provision of 25 bicycle bays and provision of only 20 car bays assists in the transition of residents to other forms of transport.
Plan development in the CBACP area around a legible street network and quality public spaces.	The proposed development is interactive with the street and provides a suitable interface between the public and private realms.
Concentrate activities, particularly those that generate steady pedestrian activation, within the CBACP area.	The proposed development contains multiple dwellings on the fringe of the CBACP area, within walking distance of the mixed use areas of the CBACP area.

As demonstrated above the proposed development is entirely consistent with the objectives of the CBACP.

5.4 State Planning Policies

5.4.1 State Planning Policy No. 7 – Design of the Built Environment

State Planning Policy No.7 – Design of the Built Environment (SPP7) became operational on 24 May 2019. It is the lead policy that elevates the importance of design quality, and sets out the principles, processes and considerations which apply to the design of the built environment in Western Australia, across all levels of planning and development.

SPP7 establishes a set of ten (10) ‘Design Principles’, providing a consistent framework to guide the design, review and decision-making process for planning proposals. Refer **Appendix 5** for a Design Principles report prepared by the project architect.

5.4.2 State Planning Policy 2.10

State Planning Policy 2.10 – Swan-Canning River System (SPP2.10) provides guiding principles for future land use and development in the precincts along the river system and performance criteria and objectives for specific precincts.

The subject site is located adjacent to the Swan River Trust (SRT) Development Control Area (DCA) for the Swan Canning Riverpark. Under clause 30A (2) (ii) of the MRS, the application requires referral to the SRT which is under the responsibility of the Department of Biodiversity Conservation and Attractions (DBCA). The proposed development will have no adverse impact on the adjacent Swan Canning Riverpark or the DCA. A Geotechnical Investigation has been prepared by Douglas Partners which confirms the requirements to ensure development can occur on the site, including the proposed basement works.

The subject site is located within the Lower Canning part of the river. Planning decisions in this area are to address a number of matters as listed in SPP2.10. The proposed development does not impact on views to the river from Canning Bridge and will not degrade the visual amenity and conservation value of the natural landscape. The scale and density of the development is consistent with the future vision for the Canning Bridge area. The proposed development is considered to comply with the policy matters set out in SPP2.10.

Refer **Appendix 10** for a copy of the Report on Geotechnical Investigation prepared by Douglas Partners.

5.4.3 State Planning Policy No. 4.2 Activity Centres for Perth and Peel

State Planning Policy 4.2 – Activity Centres for Perth and Peel (SPP4.2) is the state planning policy for the planning and development of activity centres throughout Perth and Peel. The main purpose of the policy is to specify broad planning requirements for the planning and development of new activity centres and the redevelopment and renewal of existing centres. It is mainly concerned with the distribution, function, broad land use and urban design criteria of activity centres, and with coordinating their land use and infrastructure planning.

SPP4.2 identifies Canning Bridge as a District Centre. SPP4.2 sets a performance target of a minimum 20 dwellings and desirable 30 dwellings per gross hectare within 400m of district centres. The proposed development will assist in achieving the desirable dwelling target for the Canning Bridge District Centre.

5.5 Local Planning Policies

5.5.1 LPP1.20 – Canning Bridge Activity Centre Plan Density and Bonus Provisions

Local Planning Policy 1.20 – Canning Bridge Activity Centre Plan Density and Bonus Provisions (LPP1.20) was adopted by Council on 16 February 2021, and is designed to provide guidance on the application of the Bonus Provisions within the CBACP.

The provisions of the policy predominately refer to development within the M10 and M15 zones of the CBACP which are not applicable to the subject site. As addressed in this report, the proposed development is consistent with built form outcomes planned/anticipated for the subject site given its H4 zoning under CBACP. The proposed bulk and scale of the building, and its impact on the streetscape and local amenity, are commensurate with any comparable compliant development within the H4 zone. As such, the application does not pose any inconsistencies with LPP1.20.

6 Development Assessment

6.1 Canning Bridge Activity Centre Plan – Design Guideline Assessment

Part One – Statutory Section of the CBACP contains a set of Design Guidelines (**the Guidelines**). The Guidelines are statutory provisions and apply to the entirety of the area within the CBACP boundary. Development proposed within the CBACP area is considered against the objectives, desired outcomes and requirements within the Guidelines, and in accordance with the process identified in Division One of the CBACP.

Each requirement within the Guidelines represents the quantitative criteria against which developments are to be designed and assessed. Each requirement is complemented by a Desired Outcome which represents the qualitative principles against which the decision maker exercises its judgement to determine the proposal. The Desired Outcomes are based upon the guiding principles, objectives and goals of the CBACP. As identified under section 5.2.2 of this report, the proposed development is consistent with these CBACP objectives.

An assessment is undertaken within **Table 5** below which considers the specific requirements of the Guidelines which are applicable to the proposal. Elements which are not applicable to the subject site or the proposed development are not included in the assessment table.

Table 5: CBACP Design Guidelines requirements applicable to the proposal

Clause	Requirement	Proposed	Complies
1. Land Use			
1.2.3 H4 and H8 Zone of Q2	Preferred Land Uses: Multiple Dwelling, Grouped Dwelling, Aged or Dependant Person's Dwelling, Single Bedroom Dwelling, Residential Building, Recreation - Private, Recreation – Public, Home Occupation, Home Office	Multiple Dwelling	✓
1.13 Dwelling Diversity	Minimum 20% and maximum 50% one bedroom or studio dwellings.	- 4 x 3 bedroom dual key apartments - 7 x 3 bedroom apartments - 1 x 2 bedroom apartment	Performance assessment
	Minimum 40% two bedroom dwellings.		
2. Form and Mass			
2.5 Active uses	Development is encouraged which comprises active uses at podium levels or roof top spaces such as food and beverage outlets and open spaces which are accessible to the public	Rooftop garden and communal activity area provided for the residents of the apartments.	✓
3. Heights			
3.5 Building height	For buildings in the H4 Zone, notwithstanding the 4 storey height limit, no building shall exceed 16 metres above NGL.	Proposed development is 4 storeys. The overall building height is 13.7 metres plus rooftop infrastructure.	✓
4. Street Setbacks			
4.5 Street setbacks	All development within H4 Zones in Q1 and Q2 shall have a minimum 3 metre setback to street boundaries.	Minimum 2m to The Esplanade Minimum 2.2m to Helm Street	Performance assessment
4.8 Active and landscape street frontages	Where a street setback is required, the setback area shall be activated and/or landscaped.	Street setback includes landscaping and active space.	✓

Clause	Requirement	Proposed	Complies
4.9 Roof level street setbacks	Within H4 Zones (in Q1 and Q2) any structure located at roof level shall be setback a distance of 6 metres from the building edge at the front boundary.	Roof structures are setback a minimum of 7.5 metres from the building edge at the Helm Street boundary (refer assessment note 1).	✓
5. Side and Rear Setbacks			
5.6 Side and rear setbacks	Side and rear setbacks for all development within the H8 and H4 Zones shall be 3 metres for any lot which is less than or equal to 14 metres in width or shall be 3.5 metres for any lot which is greater than 14 metres in width but less than 16 metres in width or 4 metres for any lot which is equal to or greater than 16 metres in width. Setbacks do not apply to any eaves and sun shading devices.	<p>West boundary: Ground floor – 1.5m 1st – 3rd floors – 3m to projections / 4m to main wall</p> <p>South boundary: All levels – 3m to projections / 4m to main wall</p>	Performance assessment
5.8 Privacy and solar access	Provisions of privacy and solar access and overshadowing do not apply within Q1 and Q2.	Noted. Overshadowing diagrams are provided within the development plans at Appendix 4 which demonstrate a suitable level of overshadowing for the proposed development.	✓
5.10 Roof level side and rear setbacks	Within H4 Zones (in Q1 and Q2) any structure located at roof level shall be setback a distance of 2.5 metres from the building edge at the side and rear boundaries.	All roof structures are setback a minimum of 5 metres from the building edge at side and rear boundaries.	✓
9. Facades			
9.1 Street environment	Developments shall be sympathetic to the surrounding environment in composition, proportion, materials, colours and finishes. This includes responding to (not replicating) vertical and horizontal fenestration of adjoining developments and providing responses to elements within the street verge such as bus stops, parking and service infrastructure or service entrances.	<p>The proposed development comprises saw-cut feature stone and a restrained palette providing vertical treatment to the façade. The use of materials and articulation create an attractive development.</p> <p>The proposed materials, colours and finishes are considered sympathetic to the surrounding environment and have been selected to enhance the appearance of the building.</p>	✓
9.2 Glazing	Proposed development shall incorporate substantial areas of glazing on street frontages. Glazing shall comprise no less than 50% of any façade at pedestrian/ground level and where opaque signage is proposed on glazing, unimpeded clear glazing shall still comprise greater than 50% of the frontage.	<p>The ground floor is provided with more than 50% glazing.</p> <p>Full height glazing is provided along the length of The Esplanade façade, with the Helm Street facade incorporating balconies, terraces and the pedestrian entrance creating active street frontages.</p>	✓
9.3 Semi active frontages	Semi active frontages are required in all Residential Zones with a minimum of 35% of the frontage incorporating windows or doorways with passive visual surveillance of the adjacent street at ground level.	The proposed development has been designed to encourage passive surveillance, with residential balconies and terraces overlooking the street and ground floor uses providing an active street frontage.	✓
9.4 Balconies	Windows and balconies shall be incorporated into the design of developments above ground level.	Balconies range from 16m ² - 139m ² or greater with a minimum dimension of 2.4m.	✓

Clause	Requirement	Proposed	Complies
	Balconies shall have a minimum 2.4 metre depth and a minimum area of 10m ² , to encourage use.	Refer assessment note 2	
9.5 Vandalism	Developments shall be designed so as to discourage vandalism by use of materials such as sacrificial paint or architectural features to discourage inappropriate activity.	The proposed development has been designed to encourage passive surveillance, with residential balconies overlooking the street and ground floor uses providing an active frontage which will act to discourage vandalism.	✓
9.6 Pedestrian links	Pedestrian links within development sites shall be of a design that incorporates visual interest and activity including retail and food and beverage activities or civic or community spaces.	The pedestrian entrance is located fronting Helm Street, providing direct pedestrian access from the street into the subject site.	✓
9.7 Floor level	The internal floor level of any development shall, where possible, have a finished floor level no greater than 500 mm below or above the adjoining footpath or verge level to ensure interaction between pedestrians and the adjoining buildings. Development which fronts a street with differing levels should consider innovative design to meet this requirement.	1.4m above footpath level at the highest point.	Performance assessment
10. Open Space and Landscaping			
10.5 Open space	Development in the H4 Zone shall be provided with a minimum provision of 40% open space which shall be provided in shared common space at ground levels and/or shared common space on areas such as the roof.	The site has a total site area of 1,287m ² requiring a minimum of 514.8m ² of open space. The development proposes 516m ² of open space, provided in shared common spaces and open landscaping areas on the ground floor and the communal garden on the roof. Refer to Drawing No. A113 of the Development Plans at Appendix 4 .	✓
10.6 Landscaping	Where development is not proposed to all boundaries of a site, landscaping design shall be incorporated providing that such landscaping maintains openness and visibility into the development site. Landscaping in the form of hard and soft landscaping can be utilised. Water sensitive design shall be implemented for all landscaped areas	Low level landscaping and paving to the residential lobby is provided within the street setback area to ensure openness and visual permeability. Private open space is proposed for most of the remaining setback areas. The private open space will include landscaping and paved areas. Refer to Landscape Plans at Appendix 5 .	✓
10.7 Fencing	Landscaping and/or low fencing below 1.2 metres on property boundaries, where buildings are setback from the boundary, shall reinforce the separation between public and private realm.	Low level landscaping and visually permeable fencing 1.2m in height, is provided on the property boundaries to provide separation between the public and private realm.	✓
11. Sustainability			

Clause	Requirement	Proposed	Complies
11.5 Built form	<p>All new development shall be designed to maximise passive solar principles for heating, cooling, ventilation and energy conservation. East and west facing glazing shall be minimised and shading devices shall be employed to reduce heat loads within buildings and reduce the need for air-conditioning systems. All buildings shall be designed to enable access to natural light and cross ventilation.</p> <p>At a minimum, all new development within the Kintail and Ogilvie Quarters (that is the quarters within the City of Melville) shall achieve a 4-Star Green Star design rating under Green Building Council of Australia.</p>	<p>The proposed development focuses on sustainable design and energy efficiency, with optimised cross ventilation to apartments and optimised northern solar access to living areas and balconies of majority of the apartments.</p> <p>Window protrusions are also provided for western apartments to maximise northern solar access.</p> <p>East and west facing apartments are provided with shading from each level above and generous terraces.</p> <p>Natural light penetration and cross ventilation is maximised through the design of the proposed development.</p> <p>A Green Star assessment has been prepared by Cundall which details proposed development would achieve a 4-Star Green Star design rating (refer Appendix 9).</p>	✓
11.6 Environmental rating	<p>In the H4 and H8 areas, as evidence in support of compliance with the required rating, as a minimum applicants shall submit as part of their development application a report from a Green Building Council of Australia qualified consultant demonstrating that the proposal will achieve the required level of performance. In these areas (H4 and H8) any development approval granted will be conditional upon the development being designed and constructed to include the elements identified in the supporting consultants report.</p>	<p>A Green Star assessment has been prepared by Cundall which details proposed development would achieve a 4-Star Green Star design rating (refer Appendix 9).</p>	✓
15. Level Changes			
15.1 Wall treatment	<p>All proposed retaining walls shall be treated with a non-sacrificial anti-graffiti coating to discourage potential graffiti and/or be decorated in such a way as to reduce the effect of blank facades. Landscaping in front of retaining, street furniture and articulation of the wall itself may be utilised as an alternative way of treating blank walls.</p>	<p>All proposed retaining walls are to have lush cascading planting in the terraced garden beds which will serve as suitable treatment.</p> <p>Balconies and terraces on both frontages provide ample passive surveillance to the streetscape. Direct access from the eastern ground floor apartments to the street will provide an active street frontage to deter potential graffiti or anti-social impacts.</p>	✓
15.2 Universal access	<p>All development shall provide universal access in accordance with relevant codes and standards. Innovative design features for ramps are encouraged to make universal access an integral part of design.</p>	<p>Universal access provided in accordance with Australian Standards.</p> <p>The lobby is accessible to people with a disability and the lifts provide access to all floors including the roof terrace.</p>	✓

Clause	Requirement	Proposed	Complies
16. Fencing			
16.1 Fencing	All proposed fencing which is visible from a public place shall be treated in the same way as required in Clause 15.1. Fencing shall be of a high quality on both sides.	The proposed fencing is permeable and of high quality to allow activation of the street and interaction between the private and public realms.	✓
17. Public Art			
17.1 Artwork	Artwork associated with all proposed development is encouraged.	The development proposes public art pieces to provide a community interface for the development.	✓
17.2 Public art contribution	All development which is greater than \$1 million in total capital cost of development shall contribute 1.0% of the total capital cost of development to a CBACP wide public art fund. The fund is to be used solely for the development of a strategy and acquisition of public art works to be displayed within the CBACP area. Alternatively the developer may propose to provide on-site public art works which are integrated into the design of the development. Any public art proposed shall form part of the development application to be considered by the Design Advisory Group.	The design of the proposed public art pieces is proposed to be integrated into the design of the development.	✓
17.3 Public art contribution cap	Notwithstanding Clause 17.2, the total cost liability for contribution to the public art fund shall be capped at \$500,000.	Noted.	✓
18. Parking			
18.3 Car parking	Car parking for residential development in Q1 and Q2 shall be provided at a minimum ratio of 0.75 bays up to a maximum ratio of 1.0 bay for each studio or single bedroom dwelling, and a minimum ratio of 1.0 bay up to a maximum ratio of 1.5 bays for each two or three bedroom dwelling, and a minimum ratio of 1.25 bays up to a maximum ratio of 2 bays for each dwelling with four bedrooms or greater.	<p>Maximum rates: 1 two bedroom x 1.5 = 1.5 bay 11 three bedroom x 1.5 = 16.5 bays = 18 bays</p> <p>Proposed: 20 bays provided + 2 communal electric vehicle charging bays provided. = 4 bay surplus.</p>	Performance assessment
18.8 Bicycle parking	Bicycle storage/parking shall be provided for all residential development at a ratio of one bay for every dwelling within a development site, and can be comprised within storage areas required as per Clause 19.5 or in shared parking areas or both.	25 bicycle bays and 14 storerooms are provided. The storerooms can be used for bicycle parking.	✓
18.9 Basement parking	Where basement or multi-level car parks are proposed, effective screening techniques such as planting, semi-transparent fences or screens shall be used with a preference to sleeve car parking areas with active land uses as per Figure 15.	Basement car parking is proposed as part of the development. A single lane ramp is provided as an access point to minimise visual impact from the street.	✓
19. Servicing and Functionality			
19.5 Storerooms	All residential developments shall comprise an enclosed, lockable storage area, with a minimum dimension of 1.5m with an internal	14 storerooms with a minimum dimension of 1.8m and area of 4m ² are provided within the basement.	✓

Clause	Requirement	Proposed	Complies
	area of at least 4m ² , for each grouped or multiple dwelling(s).		
20. Safety			
20.1 Access	Access to and through a development shall be safe and efficient. Entrances shall be positioned so that all pedestrian movement is adequately lit and directly visible from a public space. Access to and from car parking areas and building entrances shall be adequately sign-posted with provision of good lighting to enable safe out of hours use.	Pedestrian access to the proposed development is directly from Helm Street. The entrance includes an internal and external lobby area that is distinguishable to pedestrians that provides clear sightlines to the street. The lobby area also provides direct access to the basement car park.	✓
20.2 Active street frontages	To maximise visibility and surveillance of the public environment, the incorporation of active edge uses, including those at ground level that spill out onto public space and those located at the front of a building on the first floor that enable overlooking into public space, are encouraged. Windows can be positioned to overlook pedestrian routes, provided that privacy concerns are met.	The proposed development includes multiple balconies and terraces on all floors that front The Esplanade and Helm Street. These balconies and terraces provide ample passive surveillance to the streetscape. Direct access from the eastern ground floor apartments to the street will provide an active street frontage.	✓
20.3 Public and private realm	Development shall clearly define private and public space responsibilities. The function and ownership of an area can be clarified by paving, lighting and planting. Planting shall not create concealed spaces near paths and lighting shall allow clear lines of visibility.	A visually permeable fence and low level landscaping is proposed along both street boundaries on the subject site. This provides a clear separation between the public and private realms.	✓
20.4 Street furniture	Street furniture and lighting shall be made of durable materials to a vandal-resistant design. Graffiti-resistant materials and surface finishes are appropriate at street level in all developments. Graffiti should be reduced by increased lighting and general design features which promote visibility and discourage crime.	The material of the pause bench proposed on the eastern frontage of the subject site will be considered at the detailed design stage.	✓
20.5 Lighting	Lighting proposed for all development shall be designed so as to limit the possibility of dark shadows in adjacent private and public open spaces.	Any lighting used will not cause dark shadows in private or public open space.	✓
Assessment Notes:			
<ol style="list-style-type: none"> Helm Street is assessed as the 'front boundary' as this is the primary pedestrian access point. Three dual key apartments have a of 9m² in area to the smaller section of the dwellings. As this is a second balcony, it has not been assessed as a variation and the larger balconies meet the requirements of the ACP. The balconies provide a useable and functional space for residents. 			

As demonstrated above the proposed development is consistent with the provisions of the CBACP. The proposed development is considered to meet the desired outcomes and objectives of the CBACP and warrants approval accordingly.

6.2 State Planning Policy 7.3 – R-Codes Vol 2 Apartments

6.2.1 State Planning Policy 7.3 – R-Codes Volume 2 Element Objectives

State Planning Policy 7.3 – R-Codes Volume 2 Apartments (R-Codes Volume 2) became operational on 24 May 2019. The R-Codes Volume 2 policy places an increased focus on improving design outcomes for apartments, and applies to apartments in areas coded R40 and above, and in mixed use developments and activity centres.

As a ‘performance-based’ policy, applications for development approval are to demonstrate that the design achieves the ‘Element Objectives’ listed for each of the policy’s element objectives.

The CBACP Design Guidelines have been established as an alternative to the requirements of the R-Codes Volume 2 within the CBACP area. The following elements are not replaced by the CBACP and therefore apply to the subject site:

- 3.1 Site Analysis and Design Response;
- 4.2 Natural Ventilation;
- 4.3 Size and Layout of Dwellings;
- 4.11 Roof Design;
- 4.13 Adaptive Reuse, and
- 4.18 Utilities.

Table 6 below provides an assessment of the proposal against the required R-Codes Volume 2 Element Objectives as listed above, noting the specific outcomes achieved.

Table 6 R-Codes Volume 2 Element Objectives Assessment

ELEMENT OBJECTIVE	DESIGN RESPONSE
PART 3 – SITING THE DEVELOPMENT	
3.1 SITE ANALYSIS AND DESIGN RESPONSES	
Refer to Appendix 5 for a Design Report by Collière Architecture detailing the site analysis and design responses.	
PART 4 – DESIGNING THE BUILDING	
4.2 NATURAL VENTILATION OBJECTIVES	
O 4.2.1 Development maximises the number of apartments with natural ventilation .	All apartments within the proposed development have been designed to maximise natural ventilation and contain openings in two different orientations. There are no single aspect apartments and the proposal is consistent with the Acceptable Outcomes. Full height glazing and openings are provided in all habitable rooms opening on to balconies.
O 4.2.2 Individual dwellings are designed to optimise natural ventilation of habitable rooms .	
O 4.2.3 Single aspect apartments are designed to maximise and benefit from natural ventilation .	
4.3 SIZE AND LAYOUT OF DWELLINGS OBJECTIVES	
O 4.3.1 The internal size and layout of dwellings is functional with the ability to flexibly accommodate furniture settings and personal goods, appropriate to the expected household size.	The apartment room layouts are functional, well-organised and provide a high standard of amenity. The size of all apartments and bedrooms/living rooms exceed the minimum sizes as set out in the ‘Acceptable Outcomes’ for this element. The proposed development includes a mix of two bedroom and three bedroom apartments. This offers a diversity of housing stock to meet the needs of the locality. The design and provision of apartment

ELEMENT OBJECTIVE	DESIGN RESPONSE
	<p>layouts (particularly the dual key apartment) foster diversity and accommodate a range of different residents.</p> <p>The dual key apartments offer substantial flexibility to residents. Each potential owner could have different intentions for the use of the dual key apartments. It is expected that most owners will use the 'larger' portion of the dual key apartments as their primary residence and use the smaller in a variety of ways including:</p> <ul style="list-style-type: none"> • For live-in carers for elderly residents; • For mature children requiring independence; and • For family members who are staying with the resident on a temporary basis (e.g. visiting from overseas).
<p>O 4.3.2 Ceiling heights and room dimensions provide for well-proportioned spaces that facilitate good natural ventilation and daylight access.</p>	<p>Minimum floor to ceiling heights of at least 2.9m have been achieved for all floors. This exceeds the minimum height as set out in the 'Acceptable Outcome' for this element.</p> <p>The high ceilings proposed for each apartment allow for better than standard levels of natural sunlight and winter sun ingress.</p> <p>All rooms are well-proportioned and benefit from excellent ventilation and sunlight access.</p>
4.11 ROOF DESIGN OBJECTIVES	
<p>O 4.11.1 Roof forms are well integrated into the building design and respond positively to the street.</p>	<p>The roof form of both the proposed development is flat and reflects the modern, medium rise, residential character of the surrounding area.</p>
<p>O 4.11.2 Where possible, roof spaces are utilised to add open space, amenity, solar energy generation or other benefits to the development.</p>	<p>The rooftop garden and communal activity area provided on the roof terrace of the proposed development, provides a 174m communal space for the use and enjoyment of residents.</p> <p>Solar panels are proposed on roof of the building and will be orientated to where they will receive optimal sunlight.</p>
4.13 ADAPTIVE REUSE OBJECTIVES	
<p>O 4.13.1 New additions to existing buildings are contemporary and complementary and do not detract from the character and scale of the existing building.</p>	<p>Not applicable.</p> <p>All existing buildings on site are proposed to be demolished.</p>
<p>O 4.13.2 Residential dwellings within an adapted building provide good amenity for residents, generally in accordance with the requirements of this policy.</p>	
4.18 UTILITIES OBJECTIVES	
<p>O 4.18.1 The site is serviced with power, water, gas (where available), wastewater, fire services and telecommunications/broadband services that are fit for purpose and meet current performance and access requirements of service providers.</p>	<p>The site is serviced with all necessary urban services, fit for purpose and capable of meeting the needs of residents.</p>
<p>O 4.18.2 All utilities are located such that they are accessible for maintenance and do not restrict safe movement of vehicles or pedestrians.</p>	<p>All utilities are located in accessible locations for maintenance, and do not restrict safe movement.</p>
<p>O 4.18.3 Utilities, such as distribution boxes, power and water meters are integrated into design of buildings and landscape so that they are not visually obtrusive from the street or open space within the development.</p>	<p>All such utilities are to be integrated into the building design and/or landscaping, and are not visually obtrusive.</p>

ELEMENT OBJECTIVE	DESIGN RESPONSE
<p>O 4.18.4 Utilities within individual dwellings are of a functional size and layout and located to minimise noise or air quality impacts on habitable rooms and balconies.</p>	<p>Utilities within individual dwellings have been appropriately designed/sized and located to minimise noise and air quality impacts.</p> <p>Air conditioning units are located on the roof in appropriate screened enclosures.</p>

7 Performance Assessment

This section provides an assessment against the Desired Outcomes of the CBACP Design Guidelines, specifically in relation to dwelling diversity, street setbacks, side and rear setbacks, and car parking.

In making a determination on the suitability of each aspect of the proposal, regard must be given to the following:

- a) Any relevant purpose, objectives and provisions of CBACP;
- b) Any relevant objectives and provisions of the CBACP;
- c) Orderly and proper planning.

7.1 Dwelling diversity

Clause 1.15 of CBACP requires all development that contains ten or more dwellings to provide a minimum of 20% as one bedroom dwellings, and a minimum of 40% of the dwellings as two bedroom dwellings. The proposed development includes a one two-bedroom and 11 three-bedroom apartments, three of which are dual key apartments. As such, consideration is required against the ‘desired outcomes’ of the CBACP which states:

“All Quarters will comprise a mix and variety of development. Housing should be diverse and affordable, with a mix of options in all areas. Innovative land uses which support the Desired Outcome of each Quarter will be encouraged.”

Whilst the majority of apartments have three bedrooms, there is diversity within the products provided. Most pertinently, three apartments are in a ‘dual key’ arrangement. This means that whilst the apartments are intended to be sold as a single strata title, the apartments are equipped with two separate living quarters which may be occupied by a semi-independent occupant. The typical layouts are shown in **Figure 3** below.



Figure 3: Dual key apartment layouts

The dual key apartments offer substantial flexibility to residents. Each potential owner could have different intentions for the use of the dual key apartments. It is expected that most owners will use the ‘larger’ portion of the dual key apartments as their primary residence and use the smaller in a variety of ways including:

- For live-in carers for elderly residents;
- For mature children requiring independence; and
- For family members who are staying with the resident on a temporary basis (e.g. visiting from overseas).

This offers a diversity of housing stock to meet the needs of the locality. The design and provision of apartment layouts (particularly the dual key apartment) foster diversity and accommodate a range of different residents. Furthermore, a desktop analysis of recently approved developments in the Canning Bridge activity centre indicates that very few of the approved developments in the CBACP area include apartments which offer this layout. The provision of the dual key apartments in this proposed development therefore offers diversity from the existing market stock.

Owing to the above, it is considered the variation to dwelling diversity is consistent with the desired outcomes and warrants approval.

7.2 Street setbacks

Clause 4.6 of the Guidelines requires development within H4 Zones in Q1 and Q2 to have a minimum 3 metre setback to street boundaries. The proposed development is setback 2m from The Esplanade and 2.2m from Helm Street. As such, consideration is required against the desired outcomes which state:

“To ensure that the setback to buildings contributes to a distinct street character and that the form of multi-level development is sensitive to pedestrian scale. Podiums will provide an opportunity for creating a diversity of scale and form at lower levels, whilst taller elements are encouraged with setbacks comprising rooftop terraces and gardens at varying levels throughout development. Alternative means to reduce bulk and scale such as green walls and façade articulation are also encouraged. New buildings that are setback from the street boundary should not adversely affect the vibrancy and activity required to support the expected outcomes of the CBACP by creating unnecessary breaks in active frontages as per Figure 7.”

The proposed development has generally been designed to orientate the main outlook of dwellings to the east. This maximises views toward the Swan River and enhances the outlook and overall amenity of these apartments.

The orientation also ensures a minimum impact on the privacy of the adjoining properties. The proposed encroachments into the setbacks are primarily formed in order to provide improved privacy with the adjoining properties by orienting views obliquely.

Rather than setting back 3m for the length of the façade, the proposed development incorporates a varied setback along the street boundaries of Helm Street (north setback) and The Esplanade (east setback). The Helm Street frontage incorporates a total area of 6m² open space behind the setback line and a total area of 8.2m² of protruding into the setback area, which results in a net protrusion into the setback of just 2.2m². The Esplanade frontage incorporates 9.59m² of open space behind the setback line and a total area of 2.31m² protruding into the setback area, which results in a balance of 7.28m² behind the setback line (refer to Drawing No. A107 of the Development Plans at **Appendix 4** for calculations). **On average, there is more open space behind the setback line than there is building area in front of the setback line.** By articulating the facades, the development is considered to offer an improved design outcome compared with a compliant development.

Additionally, in response to the desired outcomes of the CBACP, we note the following:

- The building height is less than the permissible height under the CBACP. Articulation of the façade and the use of contrasting materials ensures the bulk and scale does not cause undue impact to the streetscape;
- The decreased setback does not detract from the passive surveillance. The development maintains excellent surveillance and interaction with both the Helm Street and The Esplanade frontages;
- Open balconies fronting the street will provide visual relief and reduce the impact of bulk when viewed from the street.

Owing to the above, the proposed street setbacks are considered to be appropriate and meet the Desired Outcomes of the Guidelines.

7.3 Side/rear setbacks

Clause 5.3 requires lots that are greater than 16 metres in width to be setback a minimum of 4 metres from side boundaries. The proposed setbacks to both the side and rear boundaries are 3m (measured to the window projections). The ground floor setback to the western boundary is 1.5m. As such, consideration is required against the desired outcomes which state:

“To provide a continuity of frontage at ground and podium levels to encourage activity whilst providing interest. To allow opportunities for tower elements to access sunlight, ventilation and view corridors throughout the area from and between multi-level developments. To ensure that development opportunities throughout the precinct are maximised. Developers should consider the amenity of the precinct by minimising overlooking and overshadowing of adjacent and adjoining properties through appropriate design response, supported by the setback provisions of this Element.”

The encroachments into side setbacks to the west and south are primarily to provide improved privacy with the adjoining properties by orienting views obliquely, rather than facing windows toward the adjoining property (refer **Figure 4** below). This has been achieved through the sculpting of windows in order ensure the main outlook from the dwellings is directed towards the street frontages to maintain privacy for the adjoining properties.

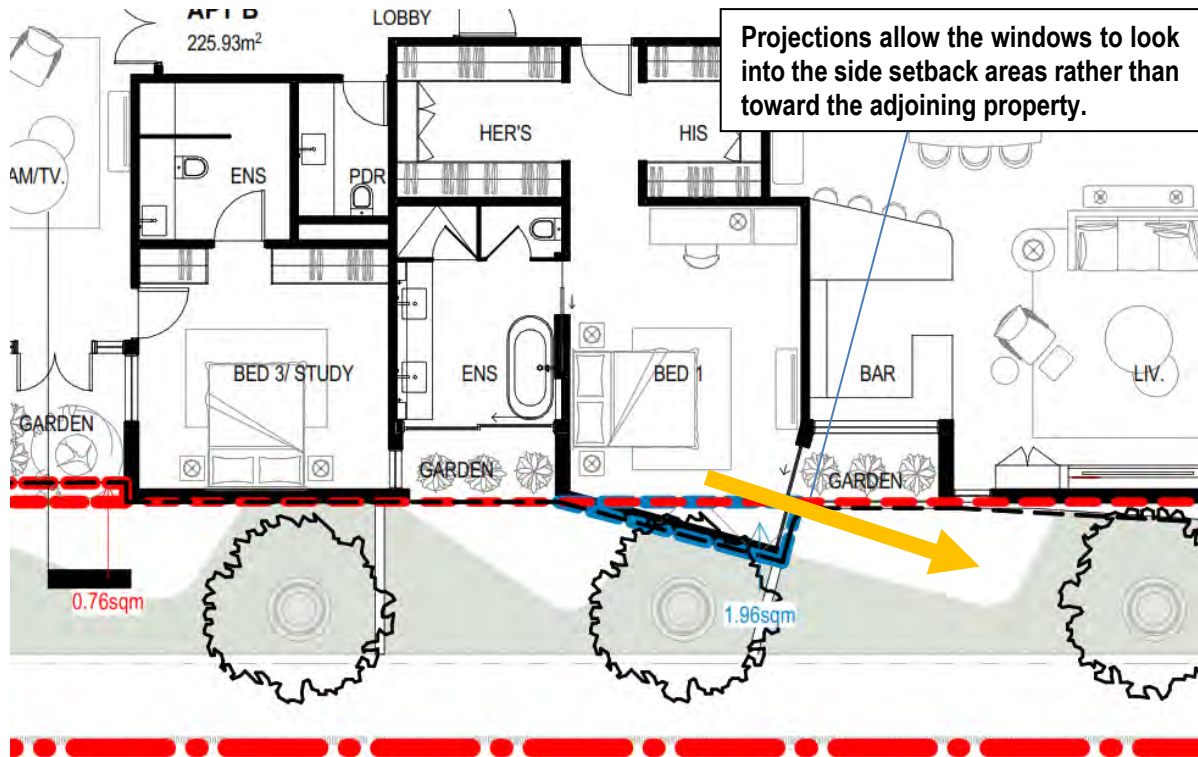


Figure 4: Window projections into the setback areas to improve privacy

On the upper levels, the proposed development incorporates only minor varied setbacks along the side boundaries on the upper floors to the west and south. The west setback incorporates a total area of 3.33m² of open space behind the setback line compared with 2.28m² of protrusions into the setback area, resulting in the average setback being greater than 4m. The south setback incorporates a total area of 2.8m² of protrusion away from the setback and a total area of 3.36m² of protrusion into the setback, which results in a net protrusion into the setback of just 0.56m². This is unlikely to be distinguishable from a compliant setback (refer to Drawing No. A107 of the Development Plans at **Appendix 4** for calculations).

The proposed minor variations on the upper floors to the minimum 4 metre side setbacks are considered appropriate given the encroachments into the setback areas are provided in order to ensure a greater level of visual privacy for the adjoining properties, therefore compensating for the variations. It is considered that the design provides a considerably better outcome than if the windows were to be oriented directly to the side boundaries at

the minimum 4 metres required. The proposed development has been designed to minimise overlooking and overshadowing of adjacent and adjoining properties as much as possible.

Western Ground Floor Setback

The proposed development incorporates a setback of 1.5m in lieu of the permissible 4m setback to the western boundary **at ground level only**.

The proposed variations to the side setbacks on the ground floor are considered appropriate given the encroachments into the setback are only provided on the ground floor and would not cause undue building bulk to the adjoining property. As the affected adjoining property is to the west of the subject site, there are considered to be minimal overshadowing impacts. In fact, the ground floor does not cast any further shadow than what would be cast by the upper levels.

The 1.5m setback provides adequate space for sunlight and ventilation between the subject site and the neighbouring building. It does not impact any views of significance as these are on the opposite side of the development to the east.

There are no privacy implications as the windows would be screened by a boundary fence. Additionally, a landscape trellis is provided along the western boundary setback area to provide screening for occupants of the proposed ground floor apartment. Furthermore, this creates a pleasant green interface with the adjoining property.

Finally, the development maintains the requisite open space and exceeds landscaping requirements under the R-Codes Volume 2. The reduced setback therefore does not create an ‘overdevelopment’ of the subject site.

Owing to the above, it is considered that the proposed side and rear setbacks have no undue impact to adjoining properties and for the upper levels, provide a superior design outcome compared to a compliant setback by providing improved privacy through articulation of the facades. As such, the setbacks are consistent with the desired outcomes and warrant approval accordingly.

7.4 Car Parking

Clause 18.3 requires car parking for residential development in Q1 and Q2 to be provided at a maximum rate of 1.5 bays for both two and three bedroom dwellings. This equates to a requirement of a maximum 18 bays for the proposed development. A total of **20** dedicated residential car parking bays are provided as well as two communal electric vehicle charging bays for the use of residents. It is not clear whether the communal bays are to be counted towards the ‘resident parking’ given they are not specifically assigned to residents. Regardless, the development proposes a surplus to the maximum 18 bays. As such, consideration is required against the desired outcomes which state:

“Parking is an important element to consider for development, and considerable analysis has been undertaken to respond to this need. Parking should be provided to ensure that the CBACP area can provide for its residents and guests, but should balance this need with a need to discourage private vehicle travel generally. Alternative transport is encouraged by way of providing for bicycle parking and storage, and motorcycle and scooter parking. Basement and multi storey car parks can present long blank walls to the street, or a gap with undesirable views into the basement car park, which should be avoided.”

The Desired Outcomes of the Guidelines note the importance of balancing parking demand with the need to discourage private vehicle travel generally. The proposed development has been designed to encourage alternate transportation methods by way of providing a substantial number of bicycle parking bays and storage areas, and electric vehicle charging bays.

While the number of on-site parking bays exceeds the amount typically required, this is considered necessary to cater for the 11 x three-bedroom (family sized) dwellings within the development. It also accommodates dual key apartments, which may be occupied by a separate occupant from the main dwelling. For these three apartments, it is considered reasonable to allow at least two car bays.

All residents’ parking is provided in a basement so as not to add to the bulk and scale of the development. A single access point is provided in order to limit the impact on the streetscape.

Given the above, the proposed two bay parking surplus for the proposed development is considered acceptable and worthy of approval.

7.5 Site Levels

Clause 9.7 of the CBACP requires that the internal floor level is not greater than 500mm above the adjoining footpath/verge level.

The finished floor level of the ground floor ranges from 1m to 1.4m above the footpath level of The Esplanade. As such, consideration is required against the desired outcomes which state:

“Development of the centre should respond sensitively to the site and support a sense of place. Development should be pleasing to the eye, be interactive, and provide definition between public and private spaces. Maintaining a strong urban edge with the built form and providing a variety of high quality architectural forms and features will attract people to the centre and establish a sense of place.”

The proposed finished floor levels are responsive to the site topography. The finished floor level is consistent with the footpath level at the west end of the Helm Street frontage. It has been necessary to site the ground floor at 3.1m AHD in order to accommodate the basement level and the requisite gradient of the ramp.

The design has carefully considered the level difference; rather than a 1.4m retaining wall, the proposal incorporates a tiered retaining wall with landscaping which steps down toward the street as shown in **Figure 5** below. This softens the appearance of the retaining. The 1m – 1.4m level difference allows for privacy for the ground floor courtyards fronting The Esplanade. This is particularly important on The Esplanade given the footpath immediately abuts the property boundary. Furthermore, the tiered retaining means that someone standing on the edge of the terraces is still able to look down to the footpath, maintaining passive surveillance of the street.

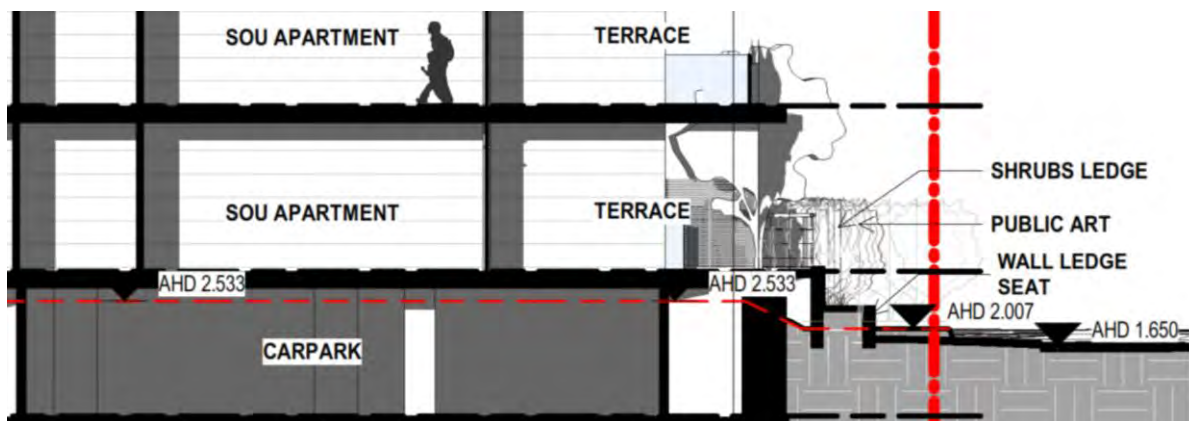


Figure 5: Tiered retaining to The Esplanade

More broadly, the design encourages informal interactions by incorporating public amenities such as artwork, seating and a dog fountain/bowl.

Owing to the above, it is considered the proposal is consistent with the desired outcomes of the CBACP and warrants approval accordingly.

8 Conclusion

The proposed development seeks to provide a four storey development comprises 12 apartments. The proposed development is situated in an area which is undergoing significant transformation and reinvigoration into a medium density and mixed use hub.

The proposed development complies with the permissible building height and seeks only minor variations to the setbacks with a view to offering a better design outcome than a compliant development by articulating and projecting walls to maximise privacy. The site is opposite an area where 10 storeys is the permissible height and the four storey scale of this development is suitable in its context.

The proposal is considered a high quality residential development within the H4 residential area under the Canning Bridge Activity Centre Plan. The design incorporates innovative dual key apartments, a rooftop terrace and provides more than the required landscaping. The proposed development responds to its location and provides a benchmark for future development which is likely to progress in the area.

In summary, the proposal is justified and considered satisfy the relevant requirements and desired outcomes of the Canning Bridge Activity Centre Plan. The variations proposed by the development are minor in nature and are appropriately justified having regard to the desired outcomes. Therefore, it is considered the proposal warrants approval.

PS Ref: 7264
City's Ref: DAP-2021-3
DAP Ref: DAP/21/01967

14 May 2021

City of Melville
Locked Bag 1
BOORAGOON WA 6954

Attention: Ben Ashwood, Senior Planning Officer

Dear Ben,

**STRATA LOT 1 (20) HELM STREET AND STRATA LOT 2 (25) THE ESPLANADE, MOUNT PLEASANT
PROPOSED FOUR STOREY MULTIPLE DWELLING DEVELOPMENT
RESPONSE TO CITY COMMENTS, AND REVISED PLANS**

Planning Solutions acts on behalf of San Brolo Pty Ltd in support of the proposed development of Strata Lot 1 (20) Helm Street and Strata Lot 2 (25) The Esplanade, Mount Pleasant (**subject site**).

The following letter addresses the comments provided by the City of Melville (**City**) in relation to the proposed development on 22 April 2021, via email. The comments sought clarification on matters relating to traffic/engineering, landscaping design, waste, and planning. The letter also addresses recommendations made by the City's Design Review Panel (**DRP**) in relation to the proposed development.

In response to the City's comments and DRP recommendations, the following plans are attached:

- i. Revised development plans are provided at **Attachment 1**.
- ii. Detailed landscape plans are provided at **Attachment 2**.

The following updated reporting also responding to the City's comments is provided at **Attachment 3**:

- i. Revised Waste Management Plan (**WMP**).

Additional information relating to traffic matters, is provided at **Attachment 4**.

The above plans, reporting, and information have been provided to address the City's comments and DRP recommendations, as required.

Response to City's comments

Detailed responses to the City's comments are provided in **Table 1** below:

Table 1: Response to comments

City comment	Applicant's response
Traffic	
<p>1. <i>The Applicant should provide a long section from existing kerb to access driveway and ramps Carpark B1 showing levels at every grade change locations the grades. It is to be noted that Clause 2.5.3(d) of Australian Standard AS/ NZS 2890.1 recommends up to 12.5% change in grade for summit grade change and 15% change in grade for sag grade to avoid the scraping of a car bottom. Clause 2.5.3(e) of Australian Standard AS/ NZS 2890.1 recommends 2.0m transitions as per Figure 2.10 of AS/ NZS 2890.1 can prevent scraping of a car bottom.</i></p>	<p>Comments from traffic engineer:</p> <p>The ramp has been revised to match the grades shown on the City's Residential Crossover Standard Drawing (Drawing 1149A3-11E refer Attachment 4).</p> <p>A ground clearance assessment has also been run on the ramp section in accordance with AS/NZS2890.1 which shows that both the B85 and B99 can clear the ramp and overhead slab at the bottom of the ramp. This is based on the standard 120mm ground clearance of a fully loaded vehicle as recommended by AS/NZS2890.1.</p> <p>Refer to Attachment 4 for the vertical clearance drawings.</p>
<p>2. <i>The first 6.0m(as a minimum) of the ramp from the property boundary into car parking area should have 1(V) in 20(H) [5%] slope in accordance with Clause 3.3(a) of Australian Standard AS/ NZS 2890.1.</i></p>	<p>Comments from traffic engineer:</p> <p>According to the City's Crossover Guidelines, the 5% grade is to allow safe disability access from the path to the property boundary. In this case, disabled users are unlikely to use this area to access the site.</p>
<p>3. <i>The Applicant should provide minimum sight lines for pedestrian safety at the interface of the front boundary and access driveway in accordance with Figure 3.3 of Australian Standard AS/ NZS 2890.1, i.e. a truncated area of 2.5m along the access driveway X 2.0m along the Helm Street frontage of the property boundary should be kept clear.</i></p>	<p>Comments from traffic engineer:</p> <p>This requirement is based on the footpath being up against the property boundary. In this case, the footpath is well separated from the property boundary and there is no path to the west of the driveway. The applicant would suggest that further truncation for pedestrian sight distance is not required in this instance.</p>
<p>4. <i>Drivers sightline from the crossover should be based on reaction time of minimum two seconds as per Main Roads WA guidelines, Section 4.1.1 of the Transport Impact Statement should be updated.</i></p>	<p>Comments from traffic engineer:</p> <p>Main Roads WA sightline requirements are applicable to intersections and driveways on roads controlled by Main Roads. The Australian Standard requirements are what we would usually apply in this case.</p>
<p>5. <i>Pause bay for the Ground Floor as shown in section 4.1.3 of the Transport Impact Statement should be located within the property boundary.</i></p>	<p>The pause bay is provided to ensure that a single access ramp to the basement car parking can be provided in order to minimise the impact of the access arrangements on the streetscape, in keeping with the established residential character of the area. The provision of a 'holding bay' is no different to a double crossover. Given the low traffic generation rates, it is not considered that two cars passing at the crossover causes safety or other traffic flow concerns.</p>

City comment	Applicant's response
6. <i>Traffic Management plan and Parking Management Plan for the construction stage of the development should be provided.</i>	The applicant has no objection to this being provided as a condition of approval.
Landscape Design	
<i>The plan provided is a concept plan only – a full detail plan will need to be provided prior to the City recommending the application for approval.</i>	Detailed landscape plans are provided in Attachment 2 .
<i>There is a discrepancy in landscape plans – three versions of landscape plans exist in the landscape , aesthetic and architectural sections of the full document. Confirmation will be required as to the correct one as some are significantly different.</i>	Detailed landscape plans are provided in Attachment 2 . The applicant confirms these are the landscaping plans to be assessed by the City.
<i>How is the glare of PV panels for roof top usage being dealt with?</i>	The landscape plans have been updated to include details proposed landscaping of the roof terrace and further details of the tree planting and species proposed. As shown in the plans, the development proposes sufficient tree planting and landscaping on the habitable areas of the roof terrace to suitably shield any residents using the space from any issues associated with glare from the PV panels. Refer to Attachment 2 for the detailed landscape plans.
<i>Street trees will need to be included along Helm street, as indicated in one of the versions of the landscape concept.</i>	Three street trees are proposed to be included along Helm Street. The proposed trees are to match the existing trees in the area. Refer to Attachment 2 for the detailed landscape plans.
Waste	
<i>Cl.2.2 of the WMP (Waste Collection): Request for private contractor to service residential waste declined. Must be serviced by City services.</i>	The proponent intends to utilise the City's waste and recycling collection services. The updated Waste Management plan included in Attachment 3 confirms this as the preferred collection method.
<i>Please provide more detail re bin store sizing. E.g. Exact dimensions. Figures provided in WMP is a bit ambiguous and I want to ensure sufficient sizing for bins and bulk waste storage allowance.</i>	Additional detail for the bin storage area and sizing has been provided in the revised Waste Management Plan included in Attachment 3 . The proposed bin storage area is of sufficient size to accommodate all receptacles. It should be noted the individual bin storage rooms have been replaced with a single bin storage area in the basement, with bins to be ferried to the verge using a 'bin tug'. This avoids the need to use of the resident lifts to transport waste.
<i>Also want displayed the proposed bin laydown area for kerbside and bulk waste collection.</i>	The bin presentation and bulk waste collection areas will be located along Helm Street. Refer to Attachment 3 for details on the exact bin presentation locations.

City comment	Applicant's response
Planning	
<p><i>Like the DRP the City remains concerned about the dual-key product. Even if the intention is for them to be used as multi-generational homes or for live-in carers the secondary apartments on each floor (both the north and south apartments) need to stand on their own two feet as acceptable apartments including minimum balcony sizes, and where possible provide suitable dwelling depth, cross-ventilation, access to natural light etc.</i></p> <p><i>The City generally agrees with the minutes of the DRP and would request design changes in response, or in lieu of design changes then suitable justification.</i></p>	<p>Refer below.</p>

Dual Key Apartments

The revised development plans contained in **Attachment 1** have taken into account the recommendations from the Design Review Panel (**DRP**) and the comments received from the City by reconsidering the approach to the dual key apartments. The proposed development plans have been revised in relation to the dual-key product with the layout of the northern apartments revised to provide suitable amenity and the southern apartments converted to traditional three bedroom apartments.

North Apartments

The northern apartment has been modified to increase the size of the 'second key' terrace and modify the layout of the living area such that it is similar to a regular apartment. Specifically:

- The door between the living area and the terrace is increased from 2.3m wide to 3.2m wide.
- The terrace increases in size from 8m² to 12.39m².

The revised design for the dual-key products allows the living areas on the 'second key' apartments to enjoy suitable light penetration from the north facing terraces, which ranges from 3 metres to 4.1 metres wide. The door between the balcony and living area is a full height glazed door and will allow sufficient sunlight to the living area. This allows an average depth for the terraces of 3.5 metres, this will enable residents to comfortably enjoy the river views and northern sun.

In summary, both keys of the northern dual key apartments now contain the requisite level of amenity that they could be independently occupied, noting the intention is that the second key would not be separately strata titled.

South Apartments

The south apartments have been revised from dual-key apartments to traditional three bedroom apartments. This has been achieved by modifying the layout of the rear (western) section of the apartment. This includes the removal of the secondary southern balcony/terrace and the reconfiguration of the main living space to a theatre/games room. A door has been maintained between the theatre/games room and the eastern portion of the apartment to enable noise isolation, temperature control and the separation of function. However, this part of the dwelling is no longer 'self contained' and cannot be considered a dual key apartment. The removal of the terrace is also considered to address any concerns about loss of privacy to the adjoining property to the south.

Response to DRP recommendations

Detailed responses to the DRP's recommendations are provide in **Table 2** below:

Table 2: Response to DRP recommendations

DRP recommendation	Applicant's response
Principle 4 – Functionality and build quality	
<ol style="list-style-type: none"> 1. Consider ways in which access to natural light, view and vista to the 'family' spaces in the dual key can be enhanced. 2. Consider exploring a reduction bedroom numbers for the dual key dwellings to establish a better outcome for the principle living space and balcony. 3. Consider reorienting the master bedroom on the eastern ground floor apartment to improve its amenity. 	Refer above.
Principle 5 – Sustainability	
<i>The Proponent is encouraged to appoint an ESD Professional and explore a coordinated response to sustainability initiatives and ways in which the project can go above and beyond Section J Compliance including the inclusion of grey water reuse.</i>	<p>An Ecological Sustainable Design report was submitted with the development application. It confirms the development is capable of achieving a 4 star Green Star rating, consistent with the requirements of the Canning Bridge Activity Centre Plan. A series of sustainable design strategies are employed throughout the development which include:</p> <ul style="list-style-type: none"> • Natural cross ventilation to all apartments; • Rooftop solar PV cells; • Double glazing; • A target average NATHERs rating of 7 stars.
Principle 6 – Amenity	
<ol style="list-style-type: none"> 1. Consider ways in which access to natural light, view and vista to the 'family' spaces in apartments 2 and 3 can be enhanced. 2. Consider exploring a reduction in apartment 2 and 3 into 1-bedroom units to establish a better outcome for the principle living space and balcony. 	Refer above.
Principle 7 – Legibility	
<i>The Proponent is encouraged to appoint an ESD Professional and explore a coordinated response to sustainability initiatives and ways in which the project can go above and beyond Section J Compliance including the inclusion of grey water reuse.</i>	The reconfiguration of the southern apartments and the removal of the service area has allowed for the creation of a more generous light well to provide natural light to the lift lobby of all levels of the building, including the ground floor. This will also assist with enhancing the legibility of the ground floor lift lobby through the creation of a suitably sized natural light source.

Refer to **Attachment 1** for the revised development plans.

We trust the responses, revised plans, and additional information address the City's queries and will assist City staff in preparing their responsible authority report (**RAR**) to the Joint Development Assessment Panel (**JDAP**).

Should you have any queries, please do not hesitate to contact the undersigned.

Yours sincerely,

pp. 

TRENT WILL
ASSOCIATE

210514 7264 Letter to City - Response to request for additional information

LEGEND

DRAINAGE

- Drainage Grate
- Side Entry Pit
- Drainage M/H

ELECTRICITY

- Power Dome
- Cable Box
- Consumer Pole
- Light Pole
- Power Pole
- Stay Pole
- Stay Wire Anchor

GAS

- Gas M/H Surround
- Gas Valve
- Gas Meter

SEWER

- Sewer Manhole
- Sewer Insp Opening
- Sewer Line

TELECOM.

- Telecom Manhole
- Telecom Pillar
- Telecom Pit

WATER

- Water Meter
- Tap
- Hydrant
- Stop Valve
- Flush Point

SURVEY

- TBM
- Peg
- Control Point
- Floor Level

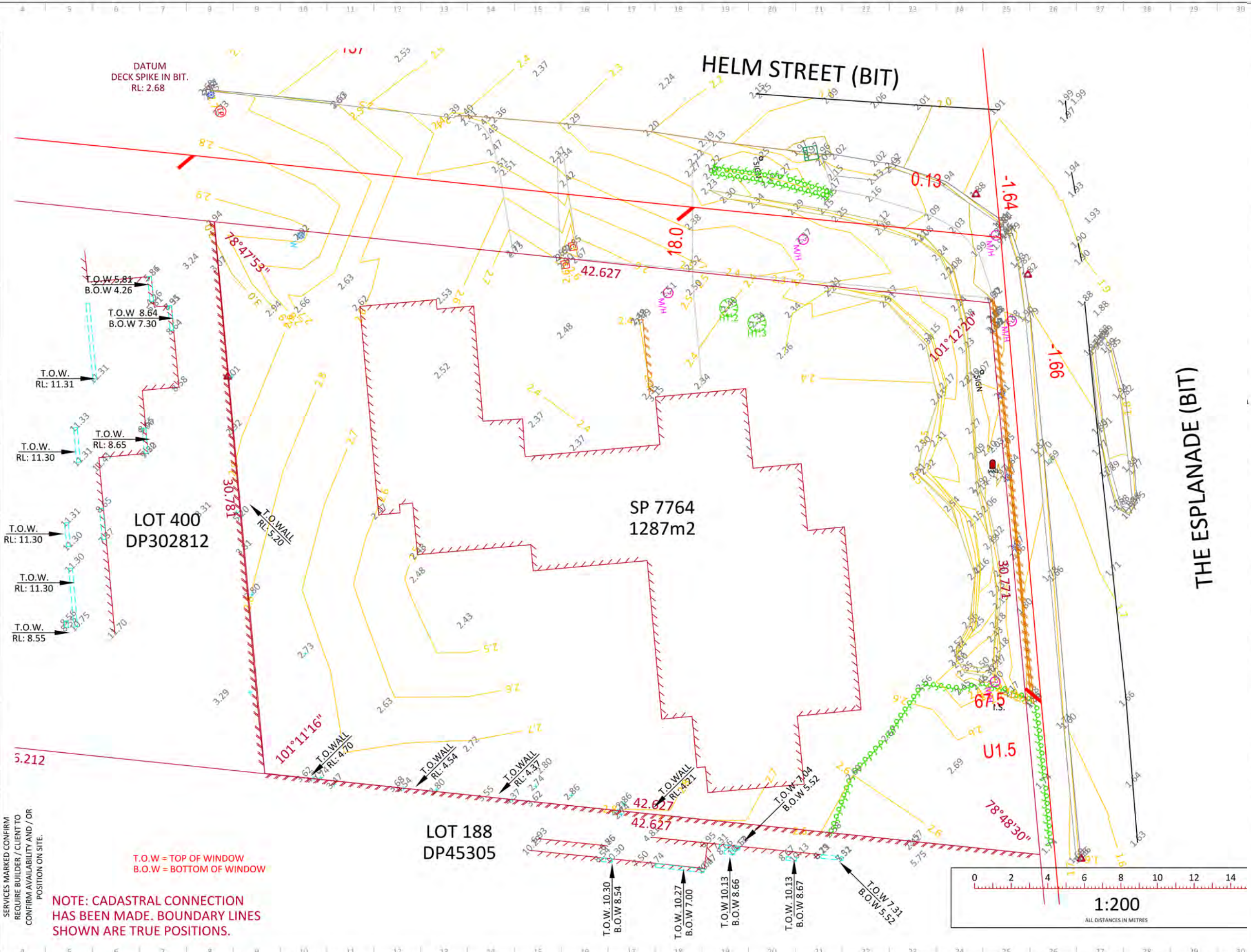
MISC.

- Bottom of Bank
- Top Of Bank
- Window

SERVICE RECORD

SERVICE	LOCATED	AVAILABLE	NO SERVICE	CONFIRM
WATER	✓			
SEWERAGE	✓			
GAS				
TEL / COMM	✓			
POWER	U/G			✓
	O/H			

SERVICES MARKED CONFIRM REQUIRE BUILDER / CLIENT TO CONFIRM AVAILABILITY AND / OR POSITION ON SITE.



CKMS

FEATURE AND CONTOUR SURVEY
LOT XX (P5) THE ESPLANADE
MOUNT PLEASANT
City of Adelaide

COLLIÈRE

ENGINEERING, CONSTRUCTION AND CIVIL SURVEYING
Subdivisions, Aerial Surveying and Photogrammetry
Page 1 of 1000 04/21
Created with AutoCAD 2014
Drawing Date: 20/03/2021
Map: A3

Malvern

Engineering, Construction and Civil Surveying
Subdivisions, Aerial Surveying and Photogrammetry
Page 1 of 1000 04/21
Created with AutoCAD 2014
Drawing Date: 20/03/2021
Map: A3

ARCHITECTURE
MASTER PLANNING
URBAN DESIGN
INTERIORS

SUITE 1 - 57A BAY VIEW TERRACE CLAREMONT WA 6010
PO BOX 1010 CLAREMONT WESTERN AUSTRALIA 6010
T (08) 9385 2858
E admin@colliere.com.au

A116- Surveyor's Drawing
25, The Esplanade, Mount Pleasant WA 6153
2010- 25 Esplanade- Development Approval
@ A3



COLLIÈRE
ARCHITECTURE



SITE:- 25 THE ESPLANADE, MOUNT PLEASANT

ARCHITECTURE
 MASTER PLANNING
 URBAN DESIGN
 INTERIORS

SUITE 1 - 57A BAY VIEW TERRACE CLAREMONT WA 6010
 PO BOX 1010 CLAREMONT WESTERN AUSTRALIA 6010
 T (08) 9385 2858
 E admin@colliere.com.au

DWG No. A101 - Existing Site Location Map
 25, The Esplanade, Mount Pleasant WA 6153
 Project No. 2010- Helm - Development Approval
 @ A3

SCALE - 1:5000



COLLIÈRE
 ARCHITECTURE



25/05/2021 5:29:30 PM



ARCHITECTURE
 MASTER PLANNING
 URBAN DESIGN
 INTERIORS

SUITE 1 - 57A BAY VIEW TERRACE CLAREMONT WA 6010
 PO BOX 1010 CLAREMONT WESTERN AUSTRALIA 6010
 T (08) 9385 2858
 E admin@colliere.com.au

DWG No. A102 - Existing Site Plan
 25, The Esplanade, Mount Pleasant WA 6153
 Project No. 2010- Helm - Development Approval
 @ A3



GRAPHIC SCALE 1:500



COLLIÈRE
 ARCHITECTURE



25/05/2021 5:29:30 PM



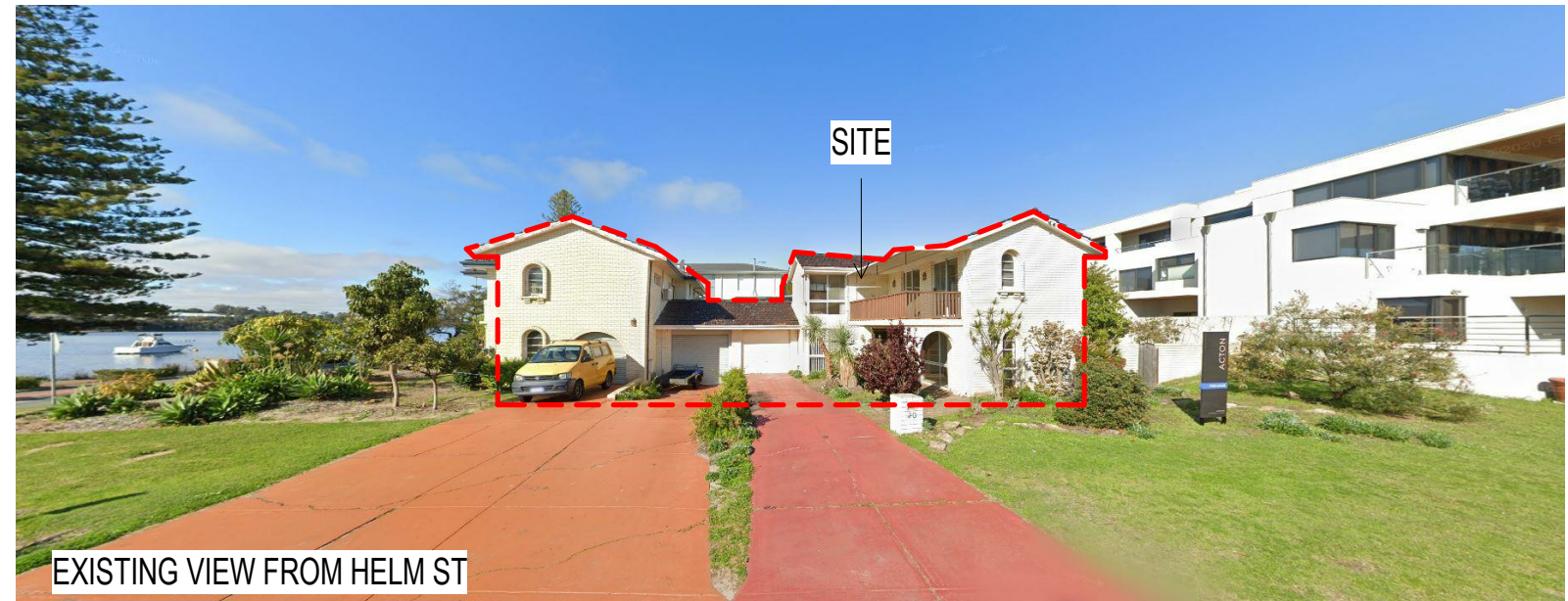
EXISTING VIEW OF THE ESPLANADE



EXISTING VIEW OF THE ESPLANADE



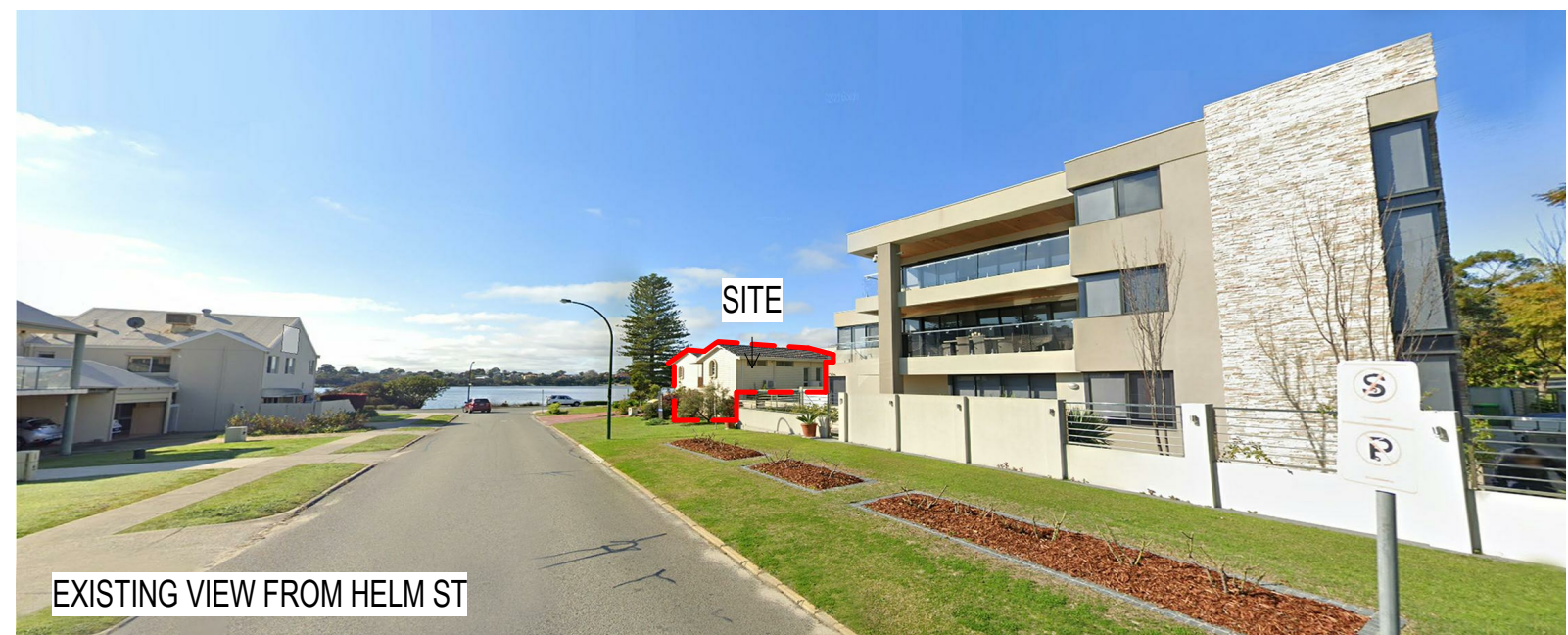
EXISTING VIEW FROM THE ESPLANADE



EXISTING VIEW FROM HELM ST



EXISTING CORNER OF 20 HELM ST, 25 ESPLANADE



EXISTING VIEW FROM HELM ST



ARCHITECTURE
MASTER PLANNING
URBAN DESIGN
INTERIORS

SUITE 1 - 57A BAY VIEW TERRACE CLAREMONT WA 6010
PO BOX 1010 CLAREMONT WESTERN AUSTRALIA 6010
T (08) 9385 2858
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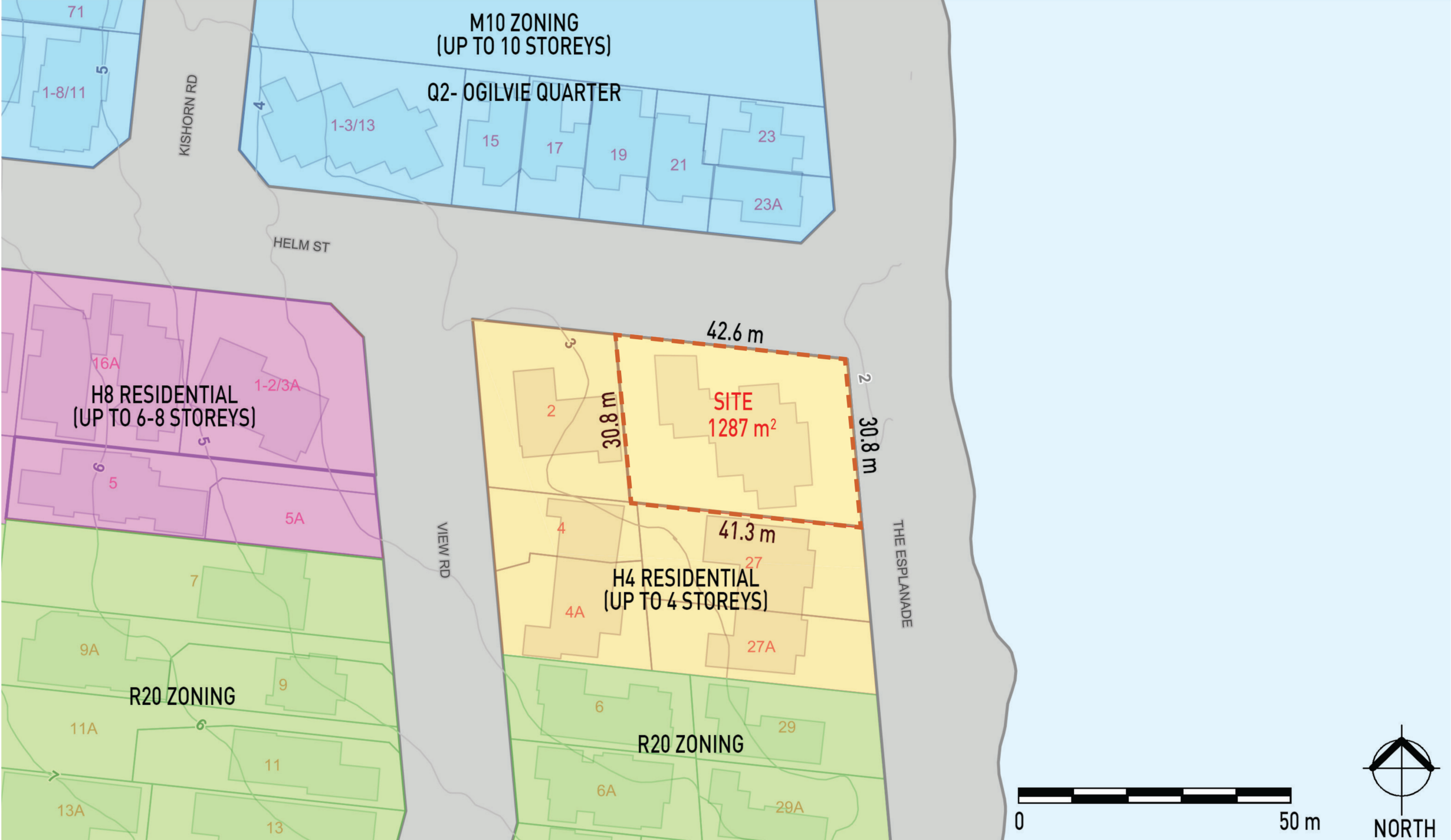
A105- Greater Context
25, The Esplanade, Mount Pleasant WA 6153
2010- Helm- Development Approval
@ A3

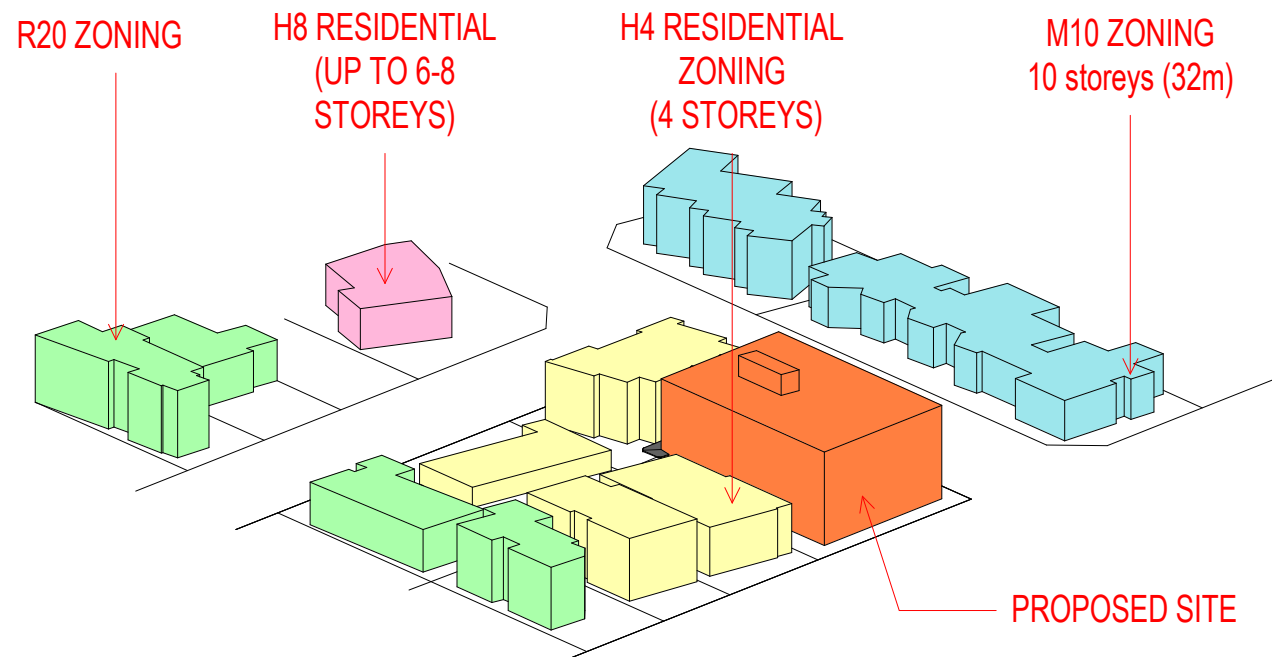


COLLIÈRE
ARCHITECTURE

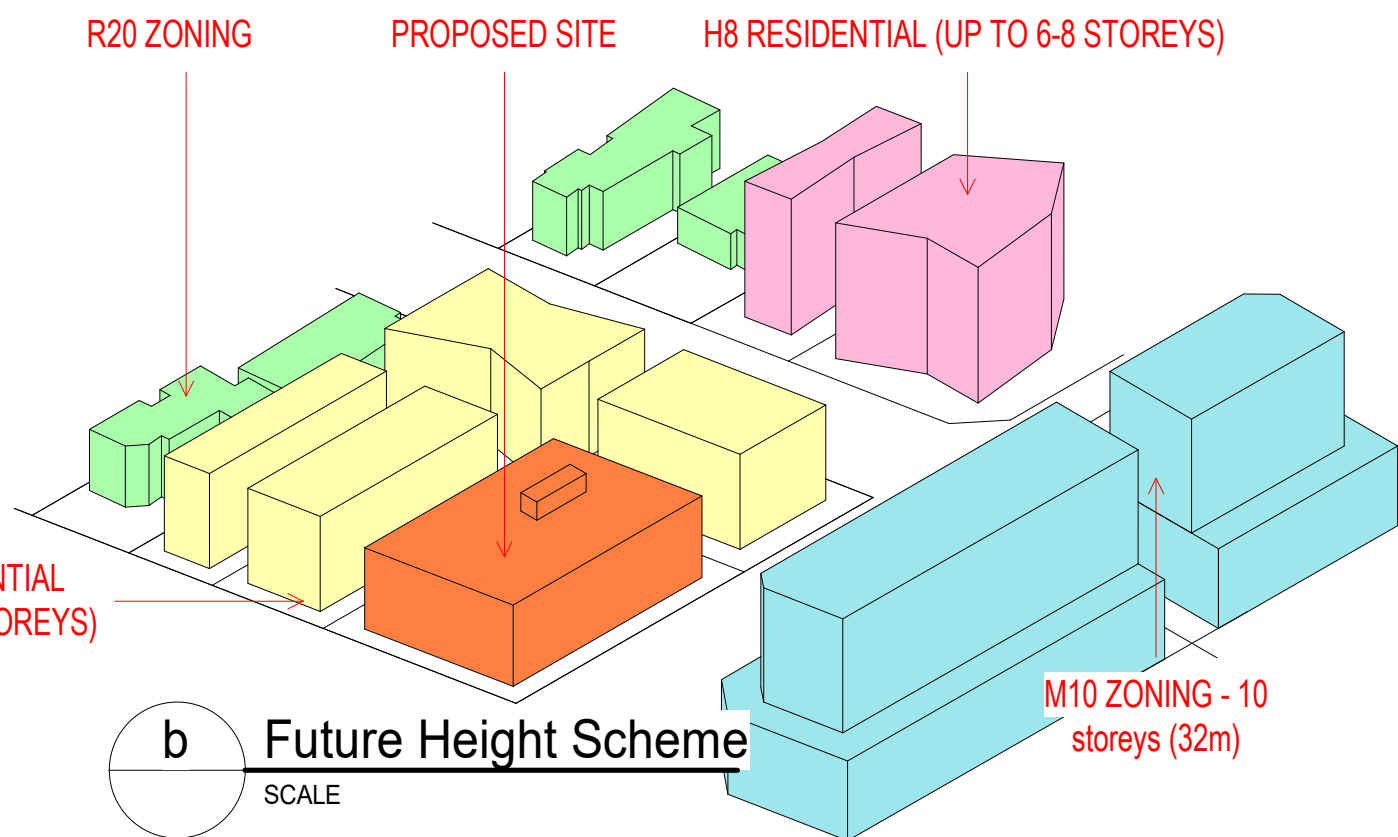


Whilst not a council notified significant site, it is an important corner that introduces H4 to the community, the landscape, art and architectural proposal recognises this.

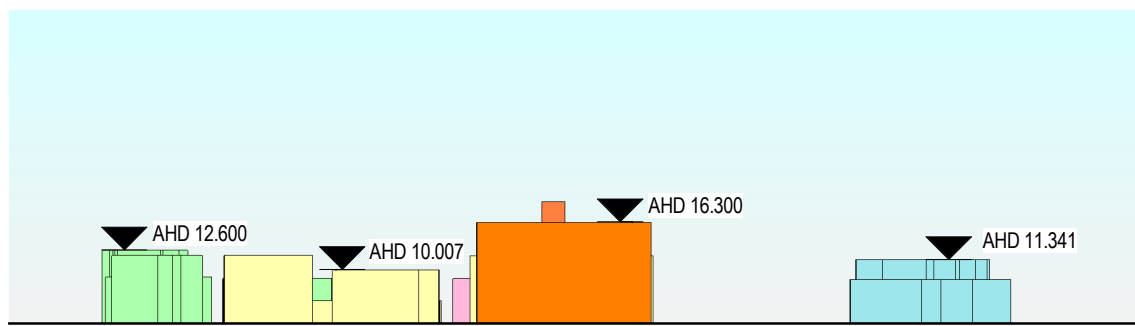




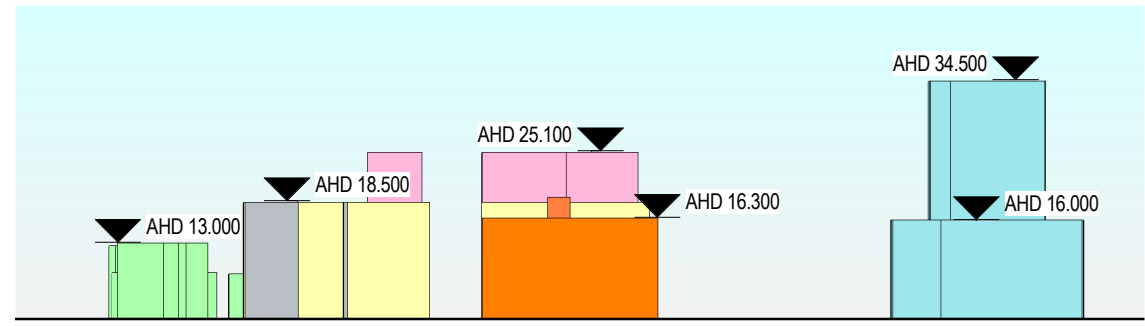
a Existing Height Scheme
SCALE



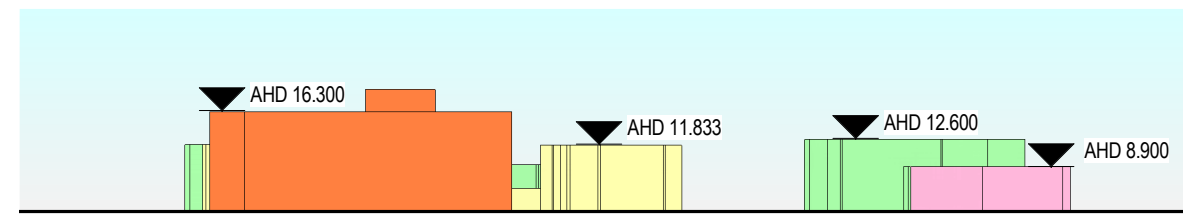
b Future Height Scheme
SCALE



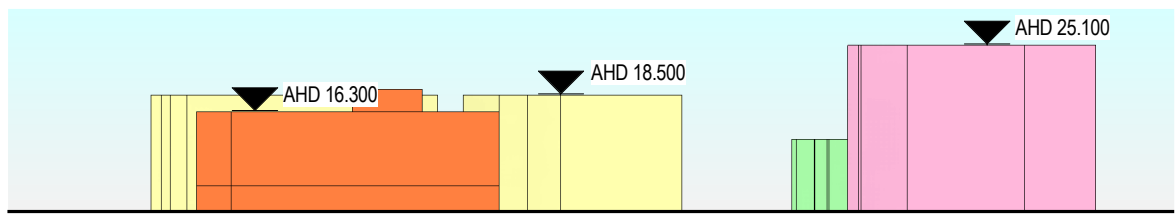
a.1 Existing East Mass Elevation
A113 SCALE 1 : 1000



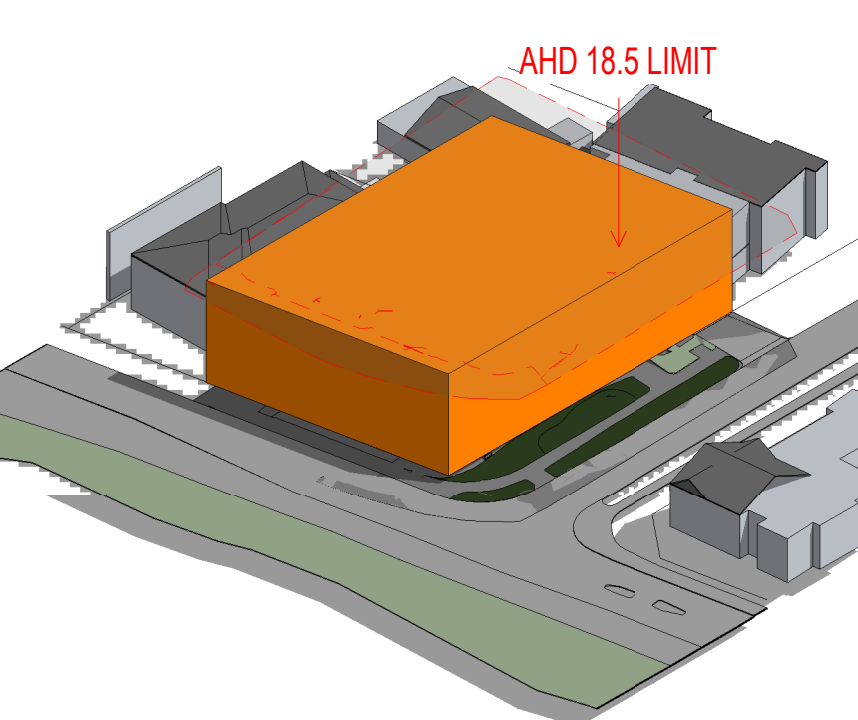
b.1 East Mass Future Elevation
A113 SCALE 1 : 1000



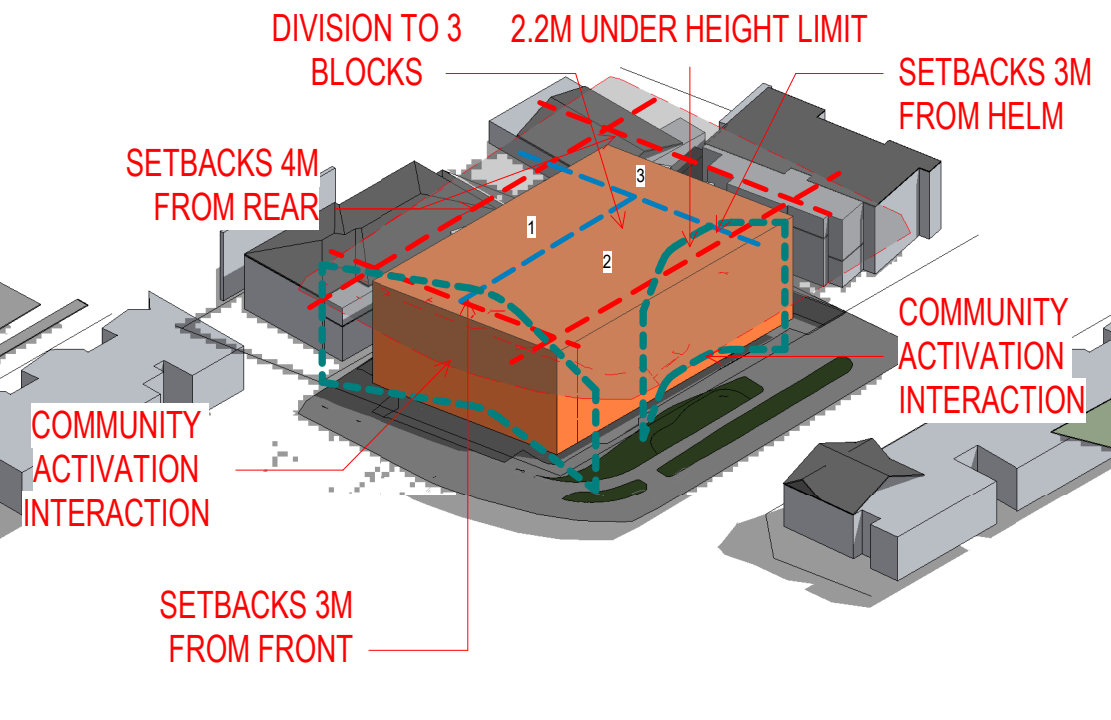
a.2 Existing North Mass Elevation
A113 SCALE 1 : 1000



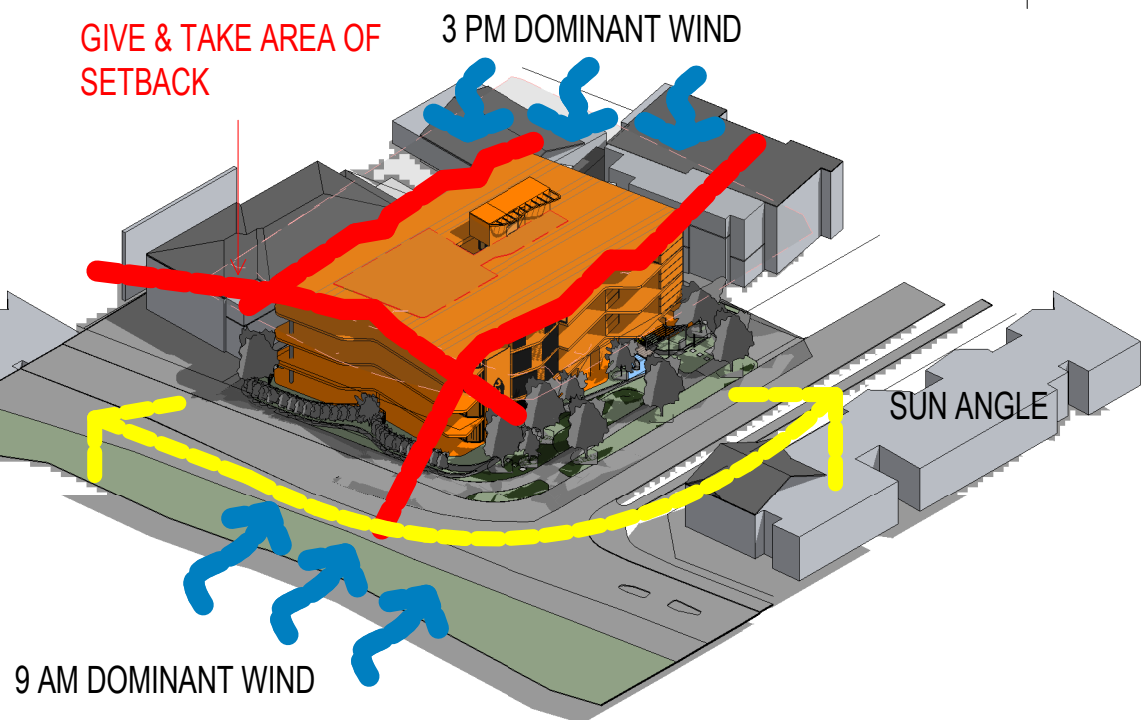
b.2 North Mass Future Elevation
A113 SCALE 1 : 1000



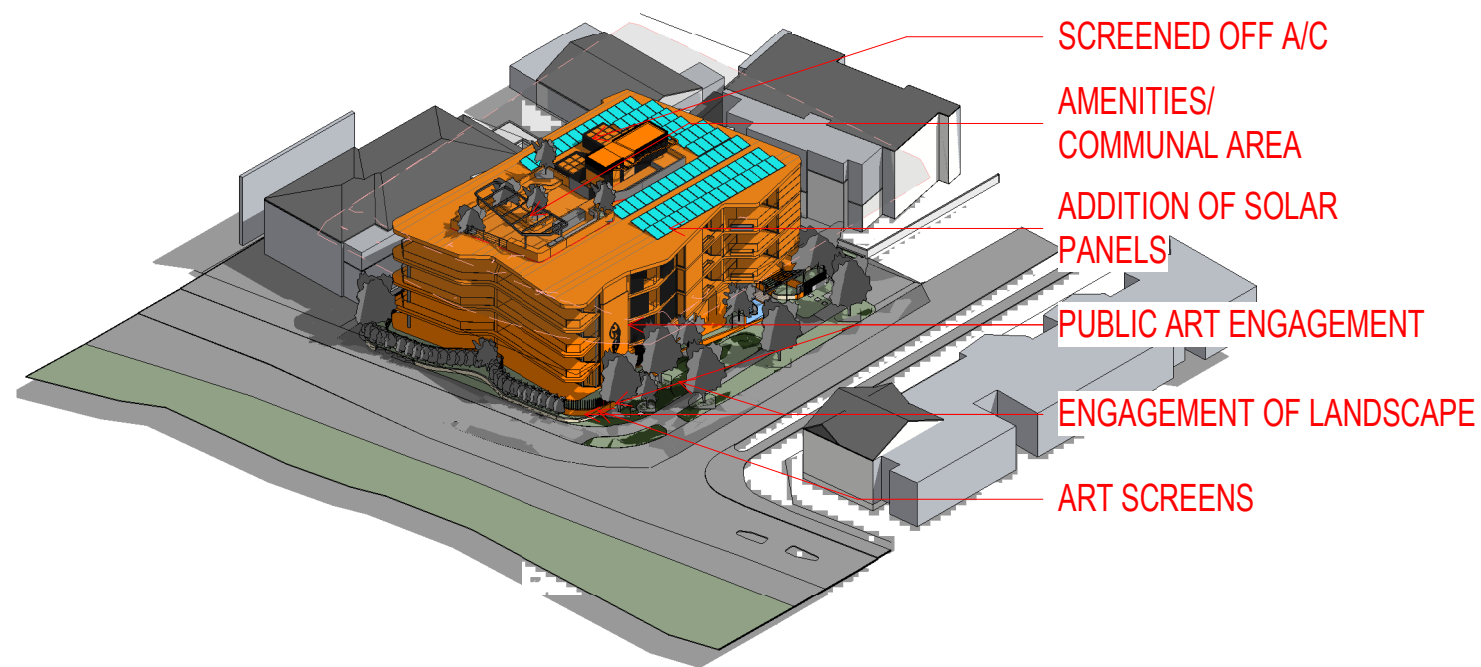
BULK MASS 16 m HEIGHT LIMIT TO COUNCIL SETBACKS



MASSING ARTICULATED INTO 3 BLOCKS. SETBACKS INCREASED TO NORTH AND EAST ELEVATIONS TO CREATE COMMUNITY INTERACTION AND HEIGHT LIMITED TO 16.3M ABOVE GROUND LEVEL.



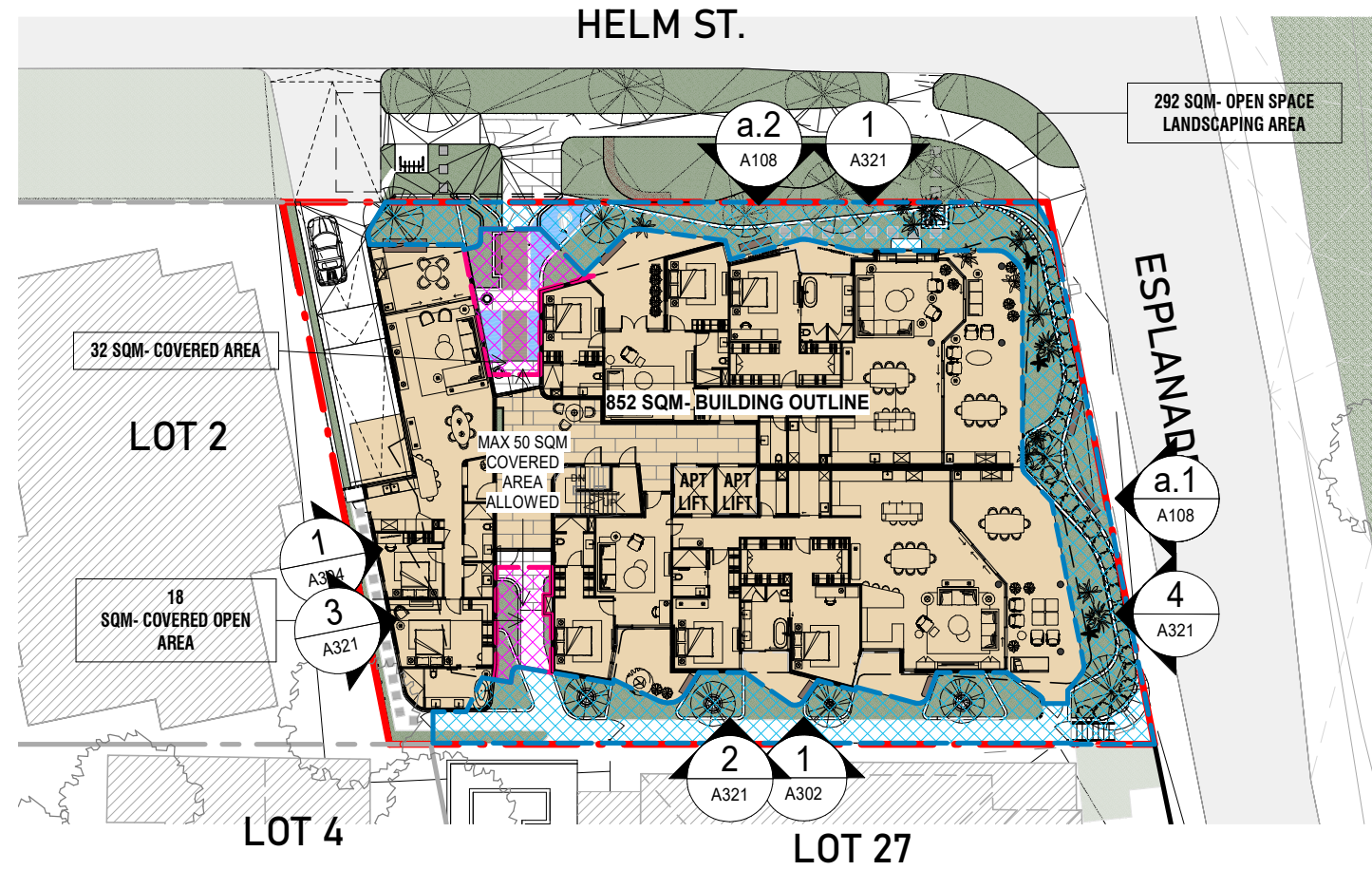
PROTRUSION OF BUILDING IN AND AWAY FROM SETBACK & MAXIMIZATION OF VIEWS, NATURAL LIGHT AND CROSS VENTILATION





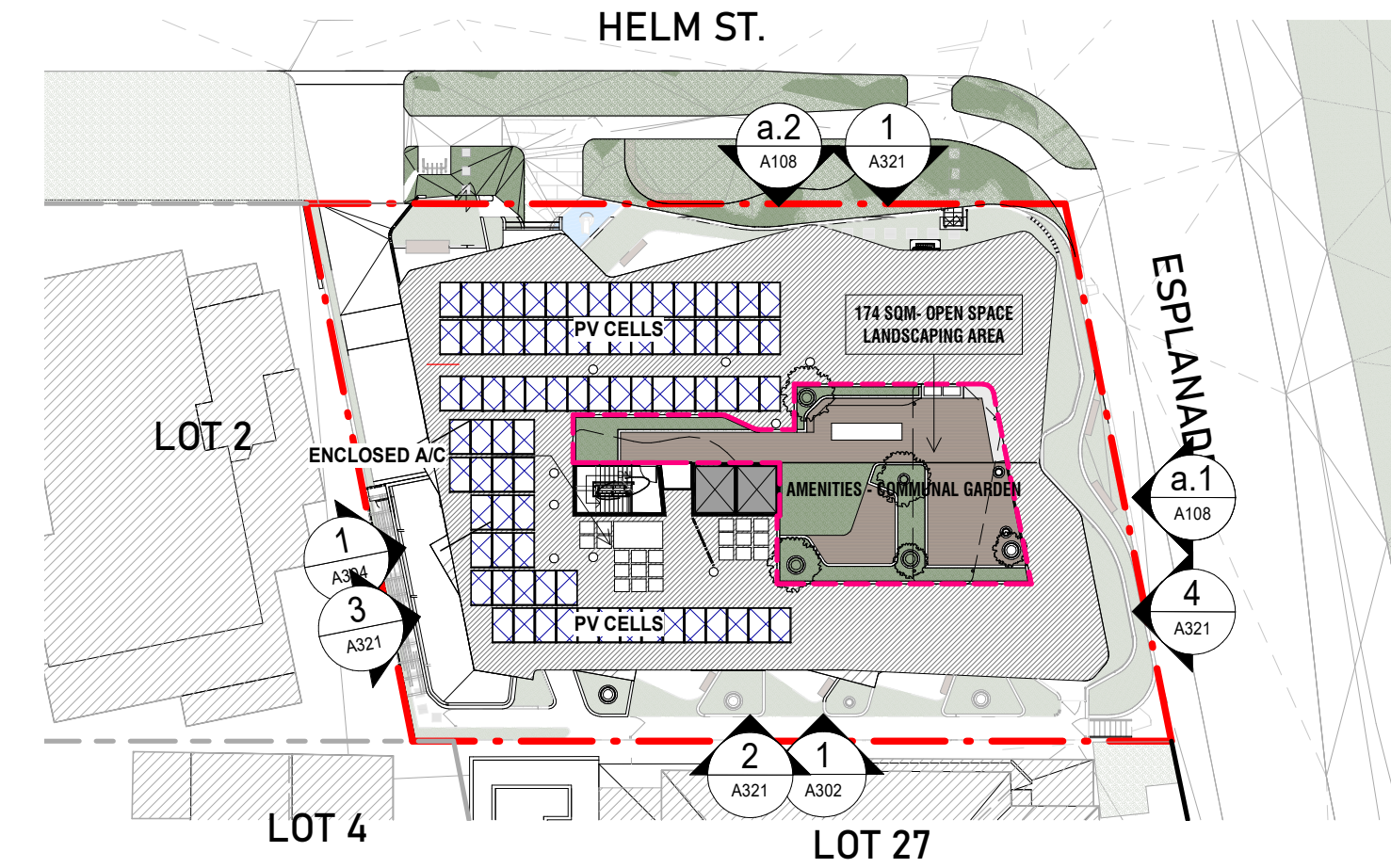
COMMUNAL ENGAGEMENT, AMENITIES AREA, SOLAR PANELS, LANDSCAPE DESIGN

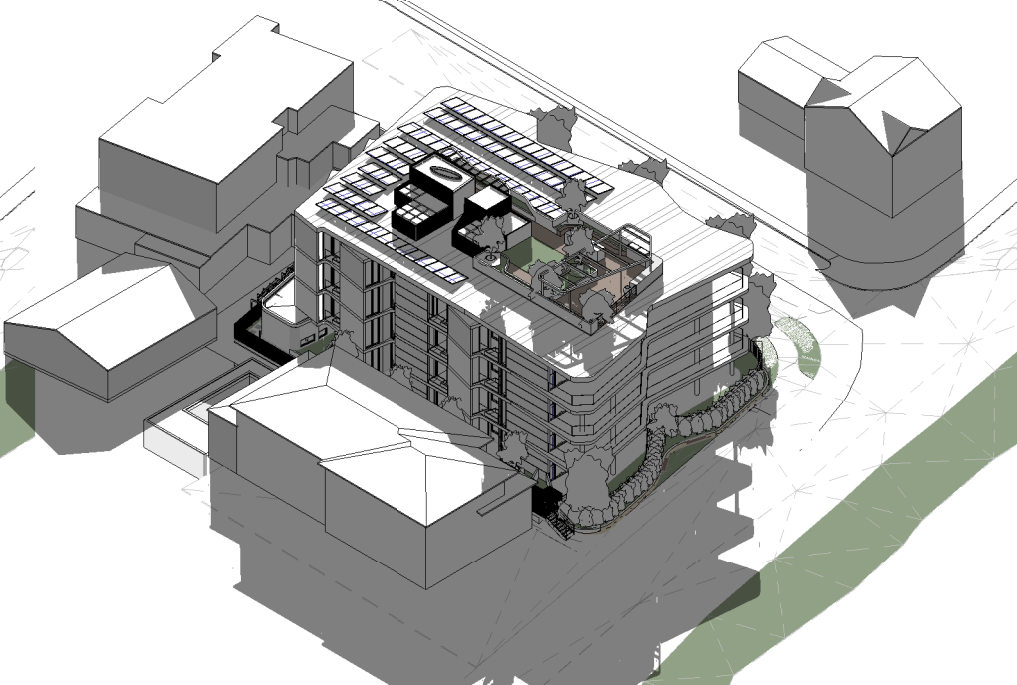
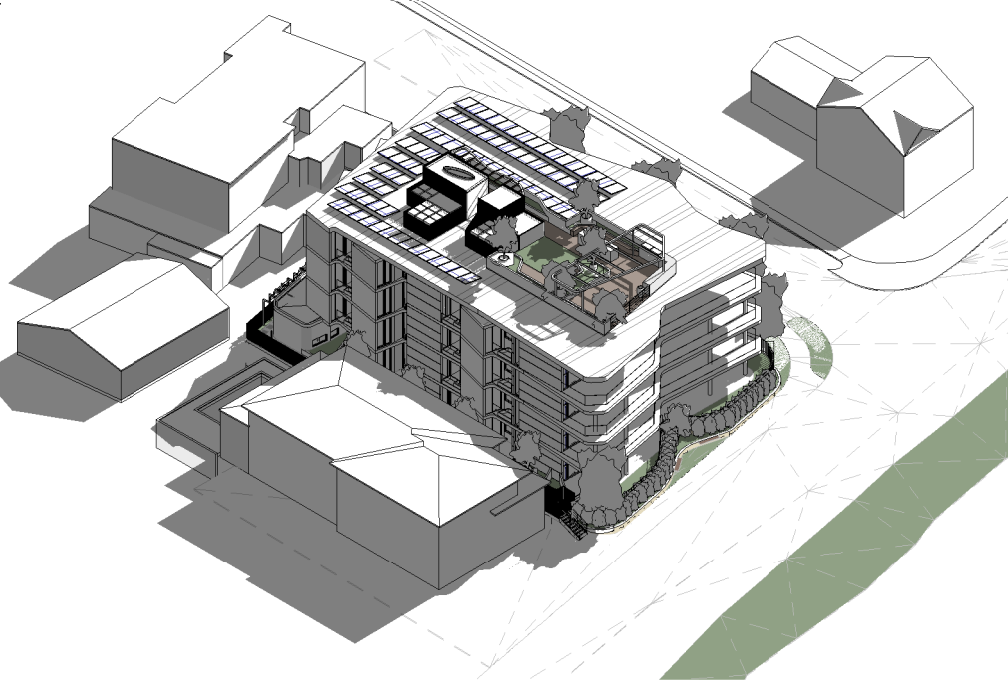
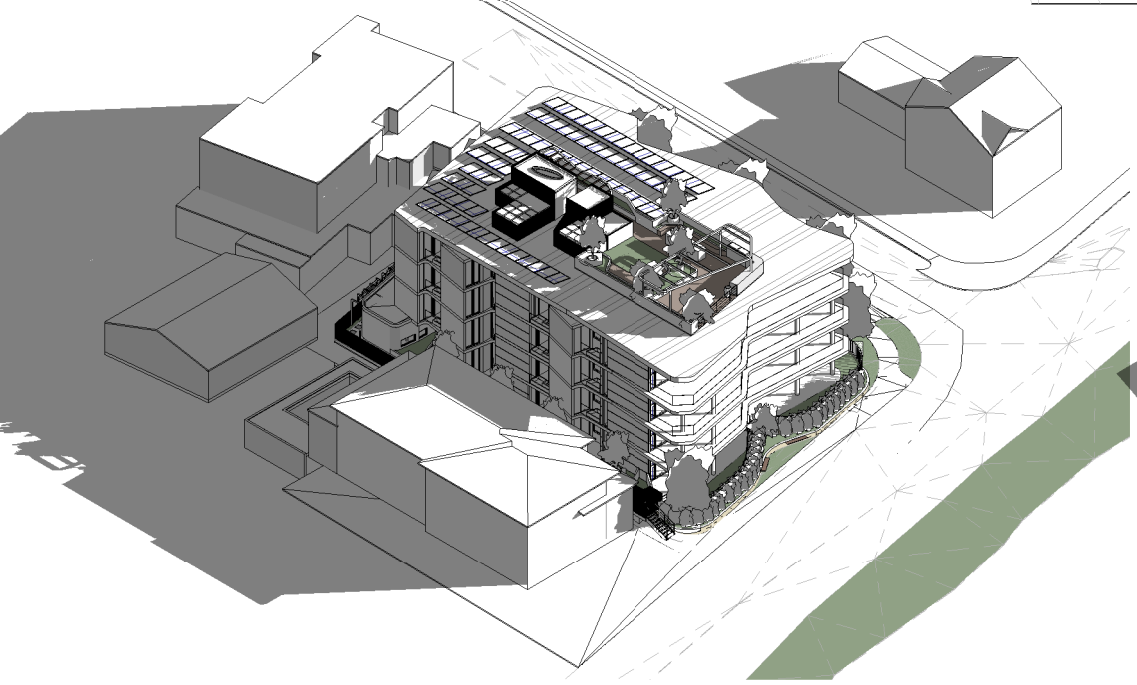
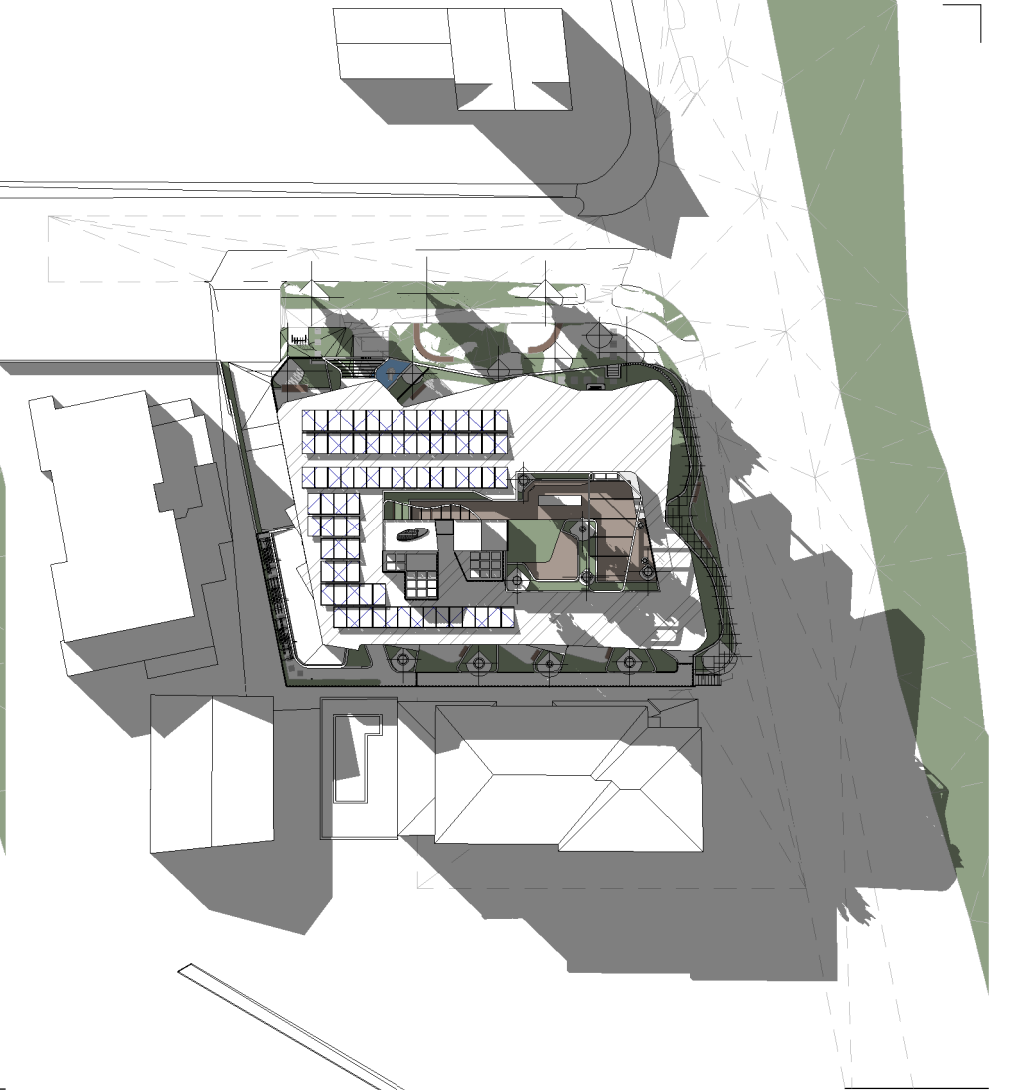
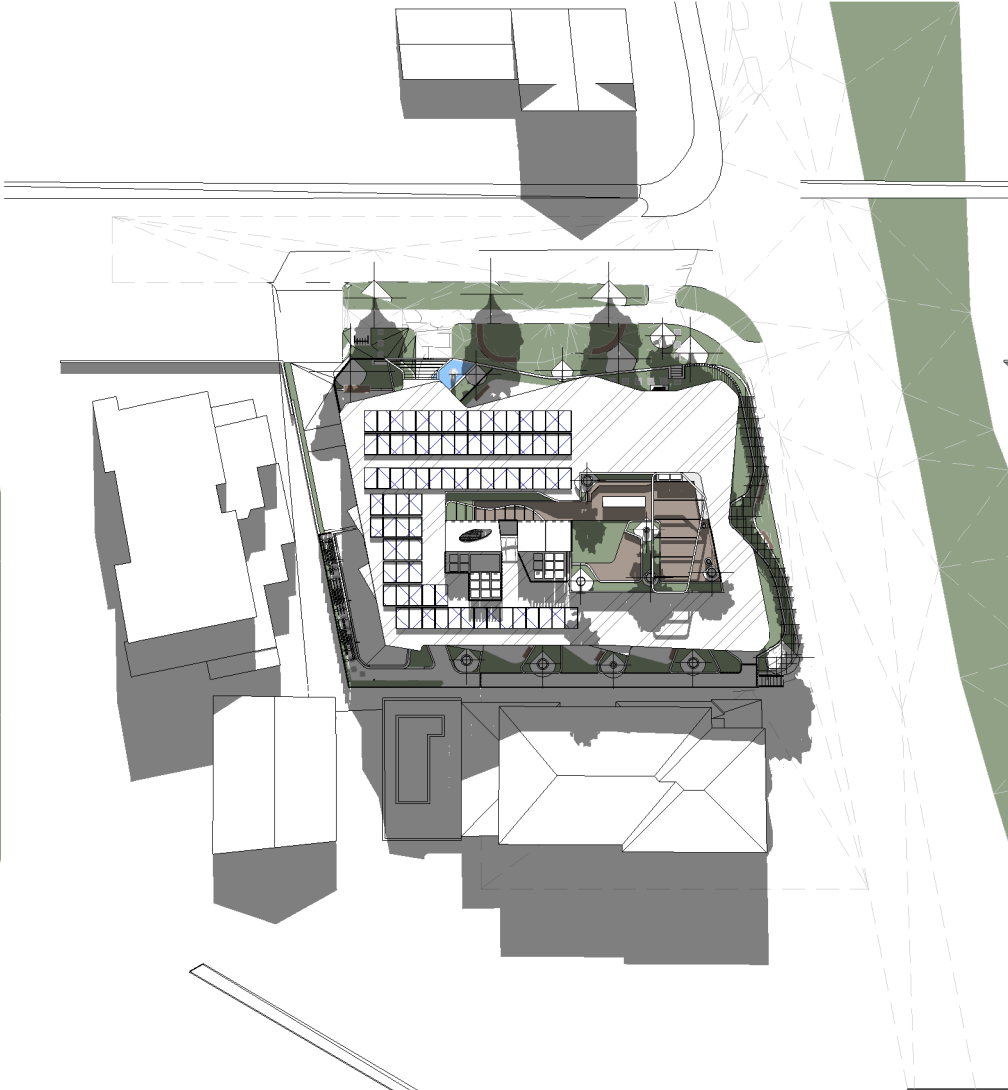
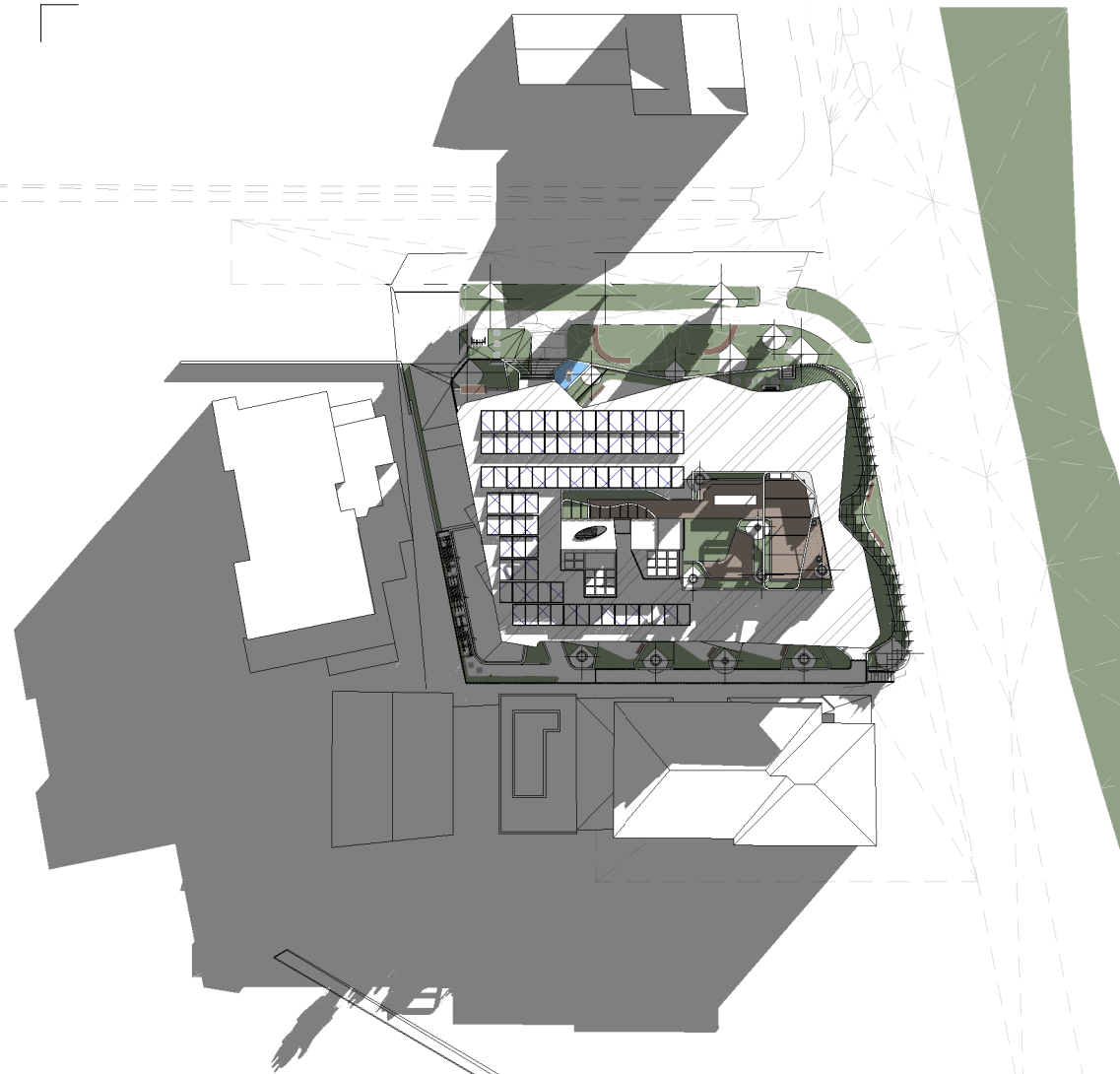


CONTEXTUAL MATERIAL SELECTIONS REFER TO 3D VIEWS FOR FINISHED MASSING PROPOSAL



OPEN SPACE COMMUNAL CALCULATION	
TOTAL SITE AREA	- 1287m ²
REQ. MINIMUM 40% OPEN SPACE	- 514.8 m ²
MAX 50M ² COVERED AREA	
TOTAL GROUND FLOOR OPEN SPACE	- 342m ²
TOTAL AMENITIES OPEN SPACE	- 174.8m ²
	AREA A - UNDER COVER - 50SQM
	AREA B - 292 SQM OPEN SPACE LANDSCAPING AREA





1 Sun Study - 21 June - 9AM
SCALE

2 Sun Study - 21 June - 12PM
SCALE

3 Sun Study - 21 June - 3PM
SCALE

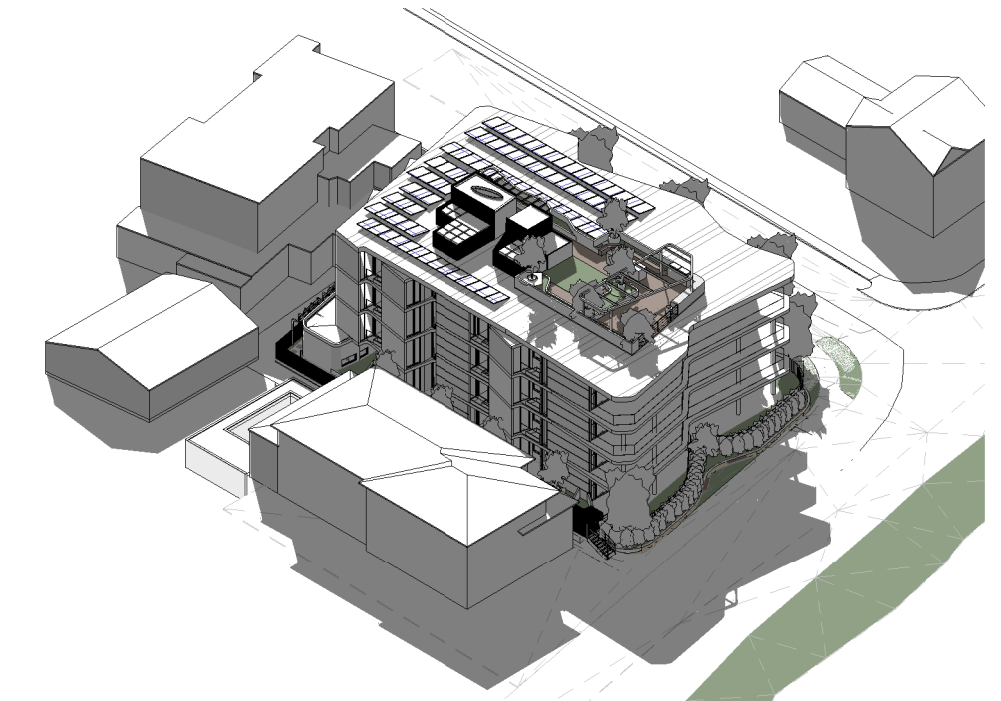
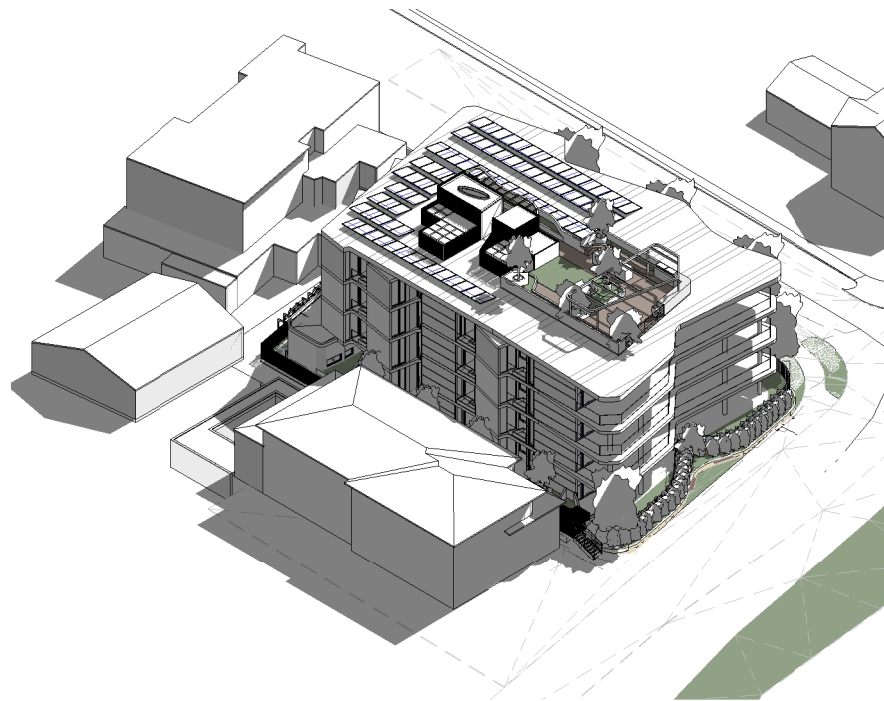
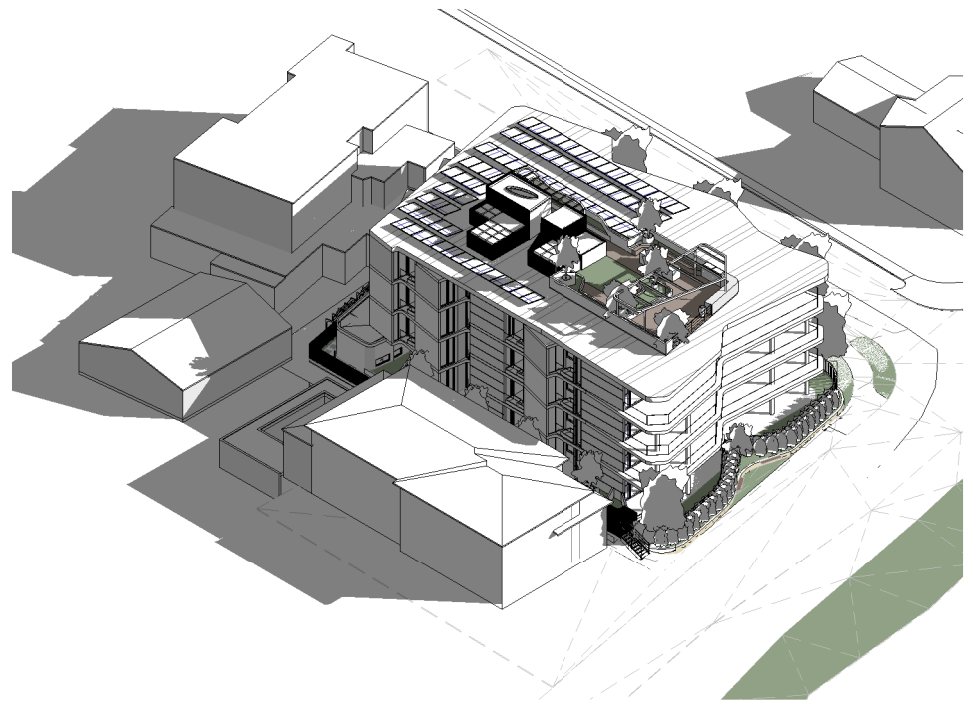
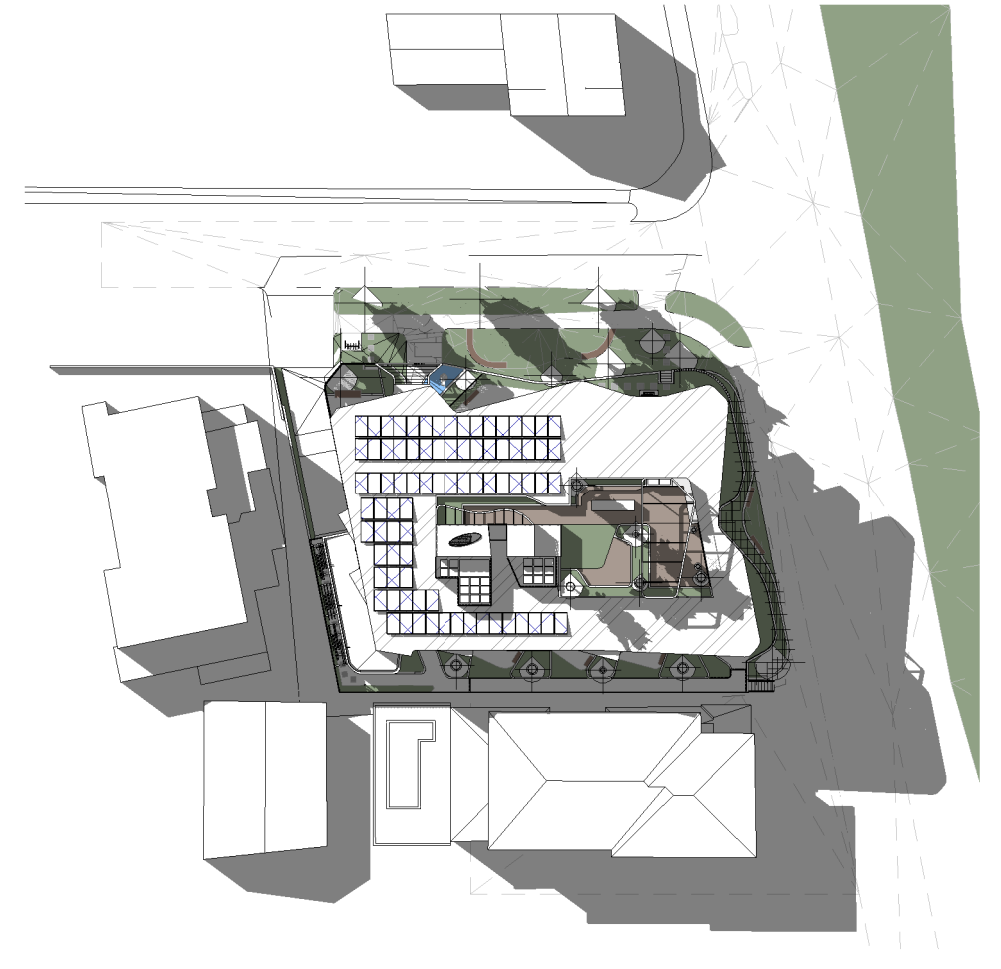
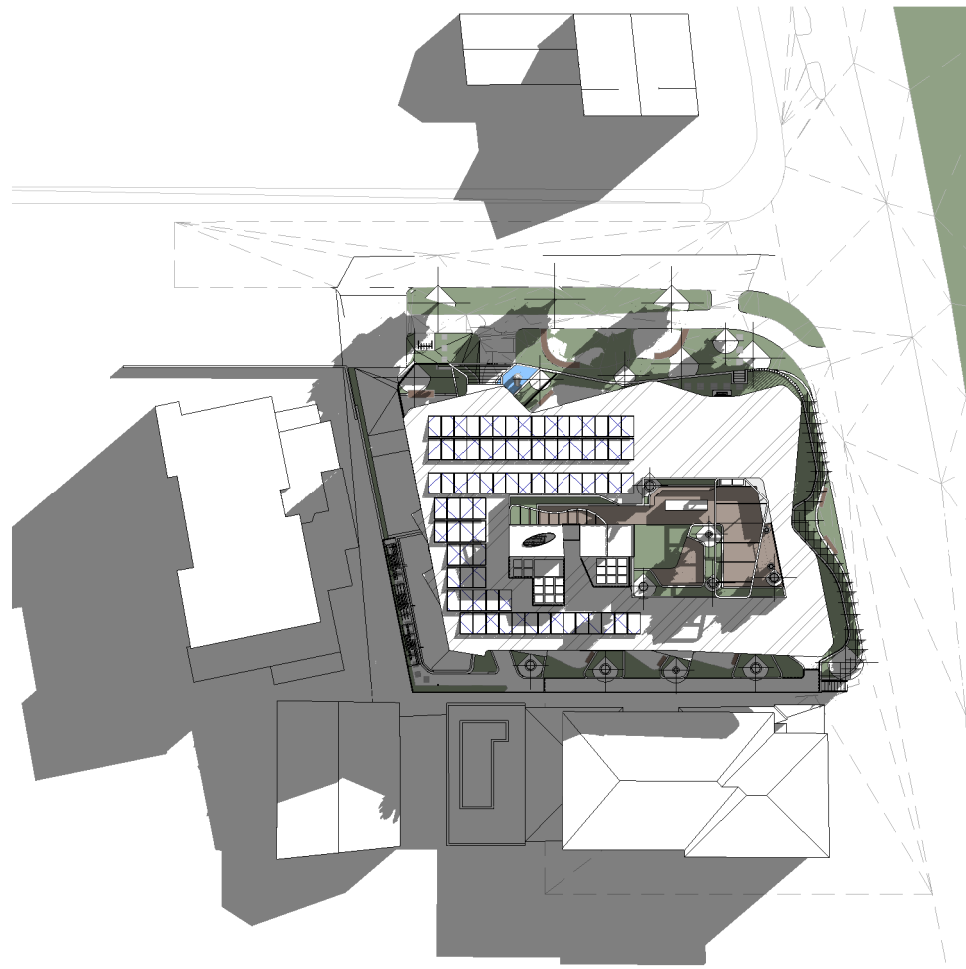
ARCHITECTURE
MASTER PLANNING
URBAN DESIGN
INTERIORS

SUITE 1 - 57A BAY VIEW TERRACE CLAREMONT WA 6010
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E admin@colliere.com.au

A451- Proposed Shadow Diagram- June
25, The Esplanade, Mount Pleasant WA 6153
2010- Helm- Development Approval
@ A3



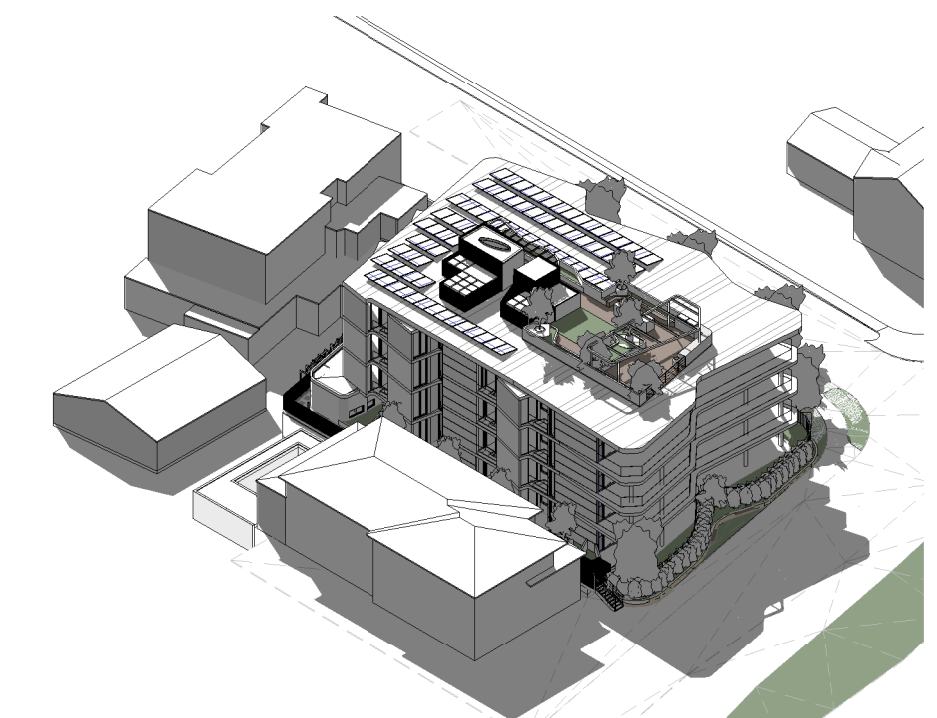
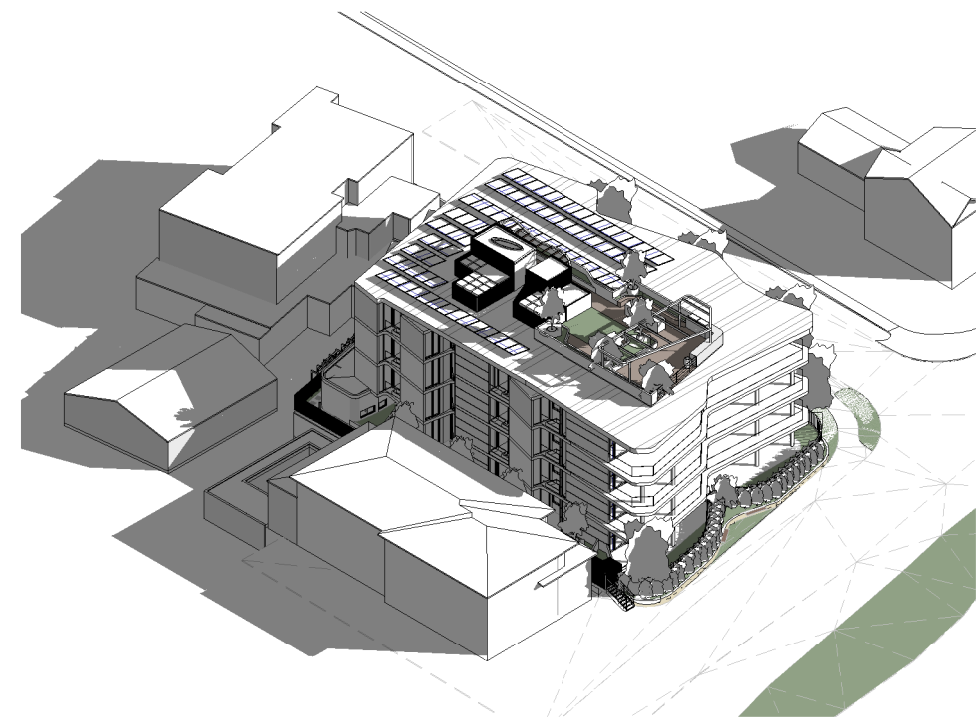
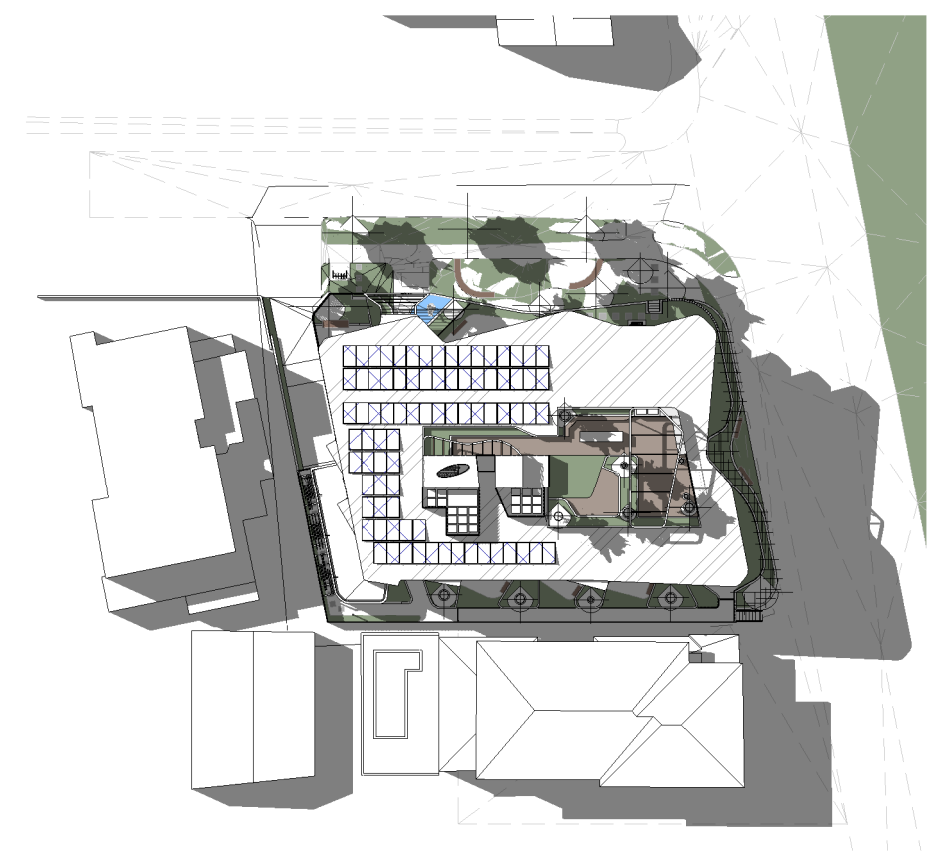
COLLIÈRE
ARCHITECTURE



1 Sun Study - 21 August - 9AM
SCALE

2 Sun Study - 21 August - 12PM
SCALE

3 Sun Study - 21 August - 3PM
SCALE



1 Sun Study - 21 September - 9AM

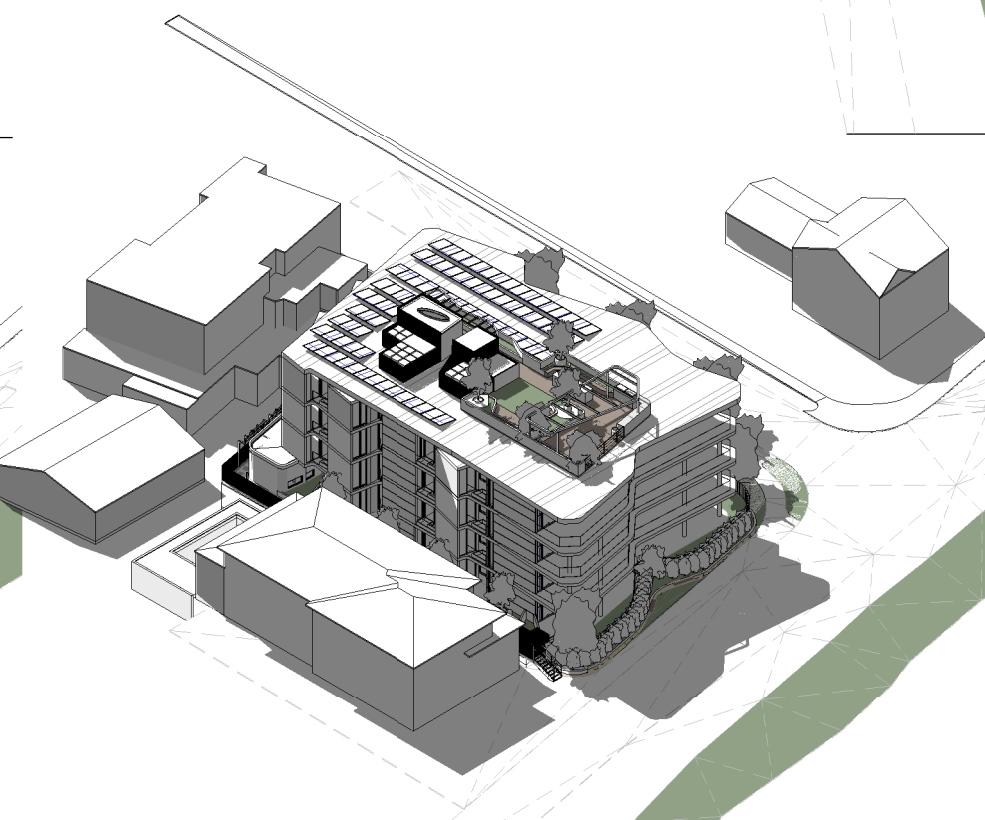
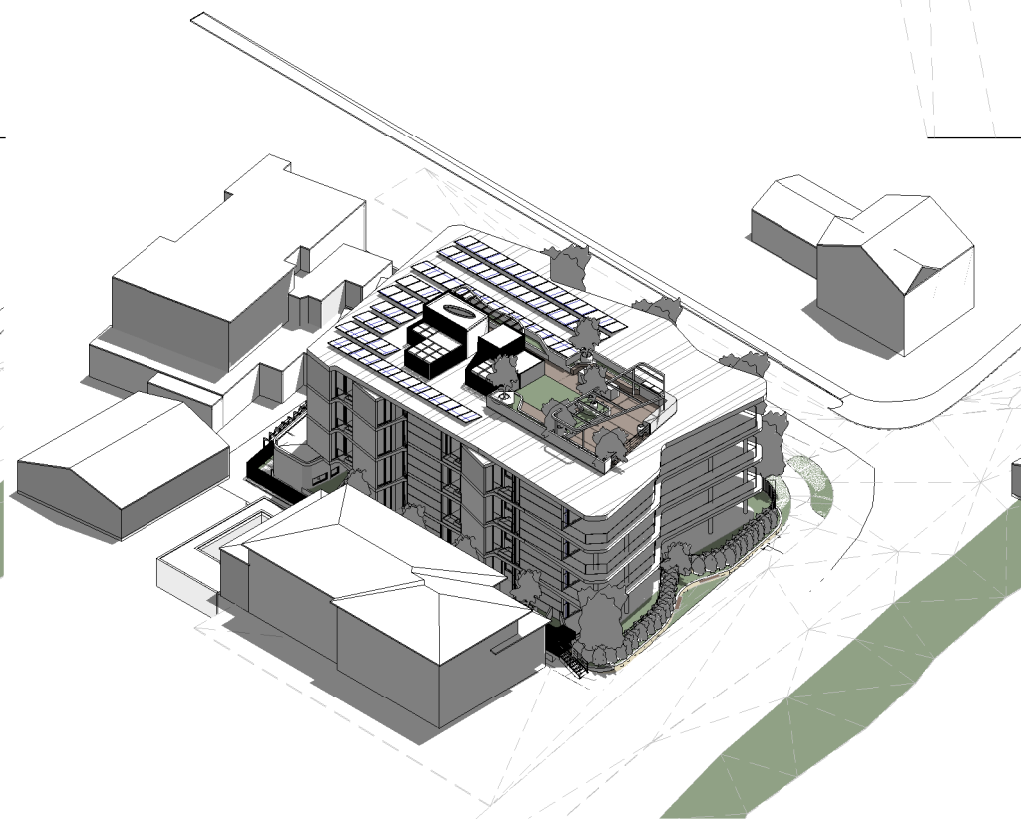
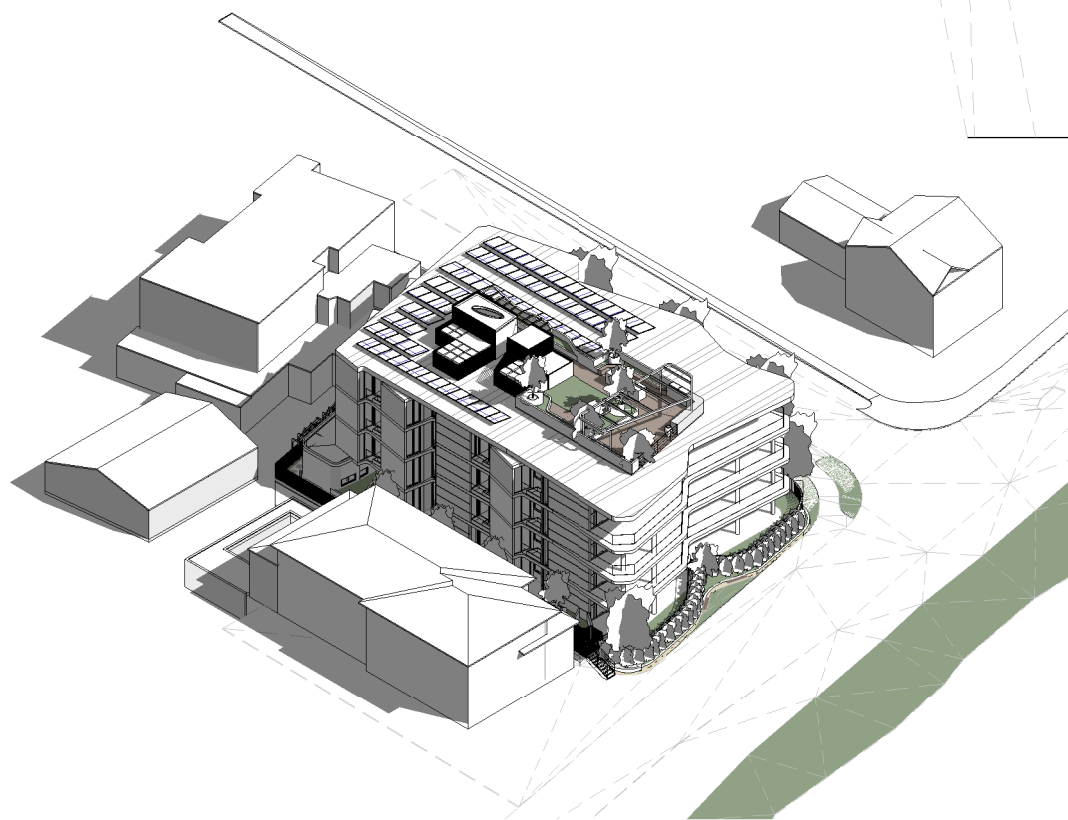
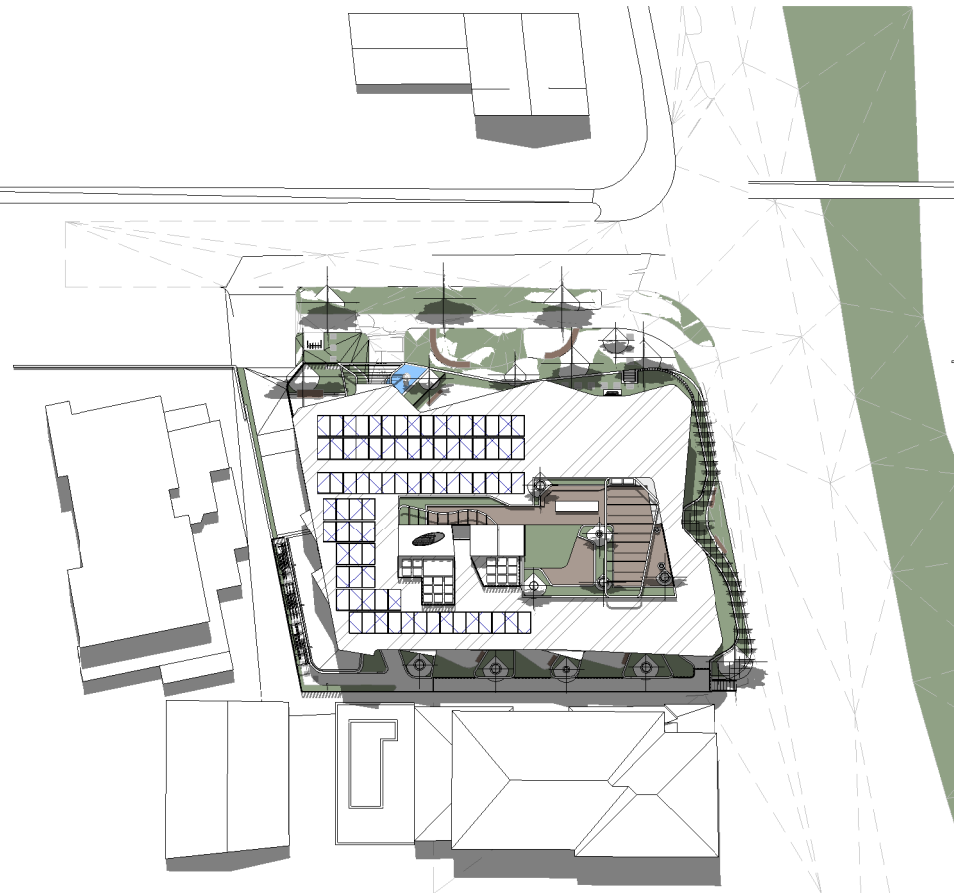
SCALE

2 Sun Study - 21 September - 12PM

SCALE

4 Sun Study - 21 September - 3PM

SCALE



1 Sun Study - 21 October - 9AM
SCALE

2 Sun Study - 21 October - 12PM
SCALE

3 Sun Study - 21 October - 3PM
SCALE

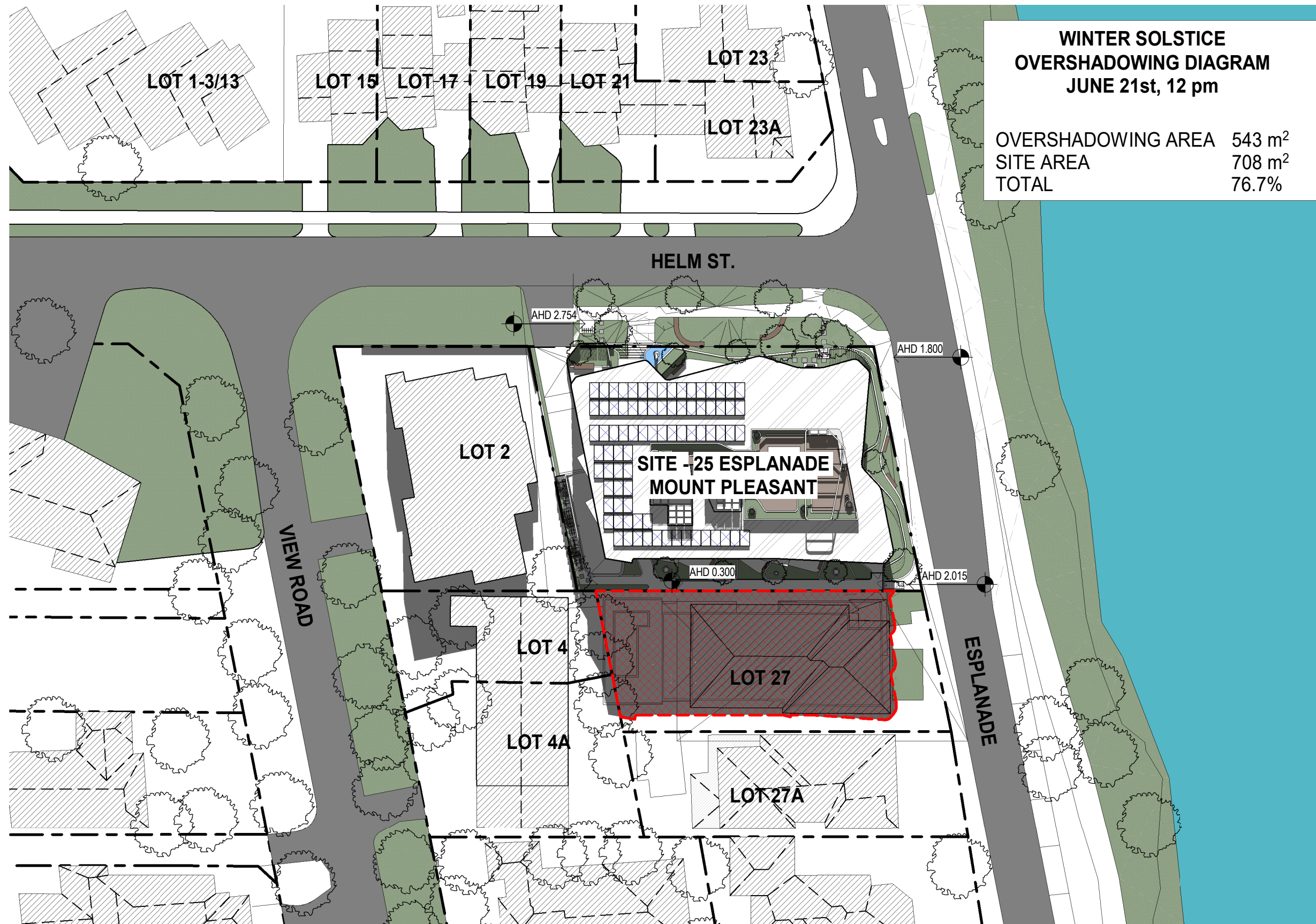
ARCHITECTURE
MASTER PLANNING
URBAN DESIGN
INTERIORS

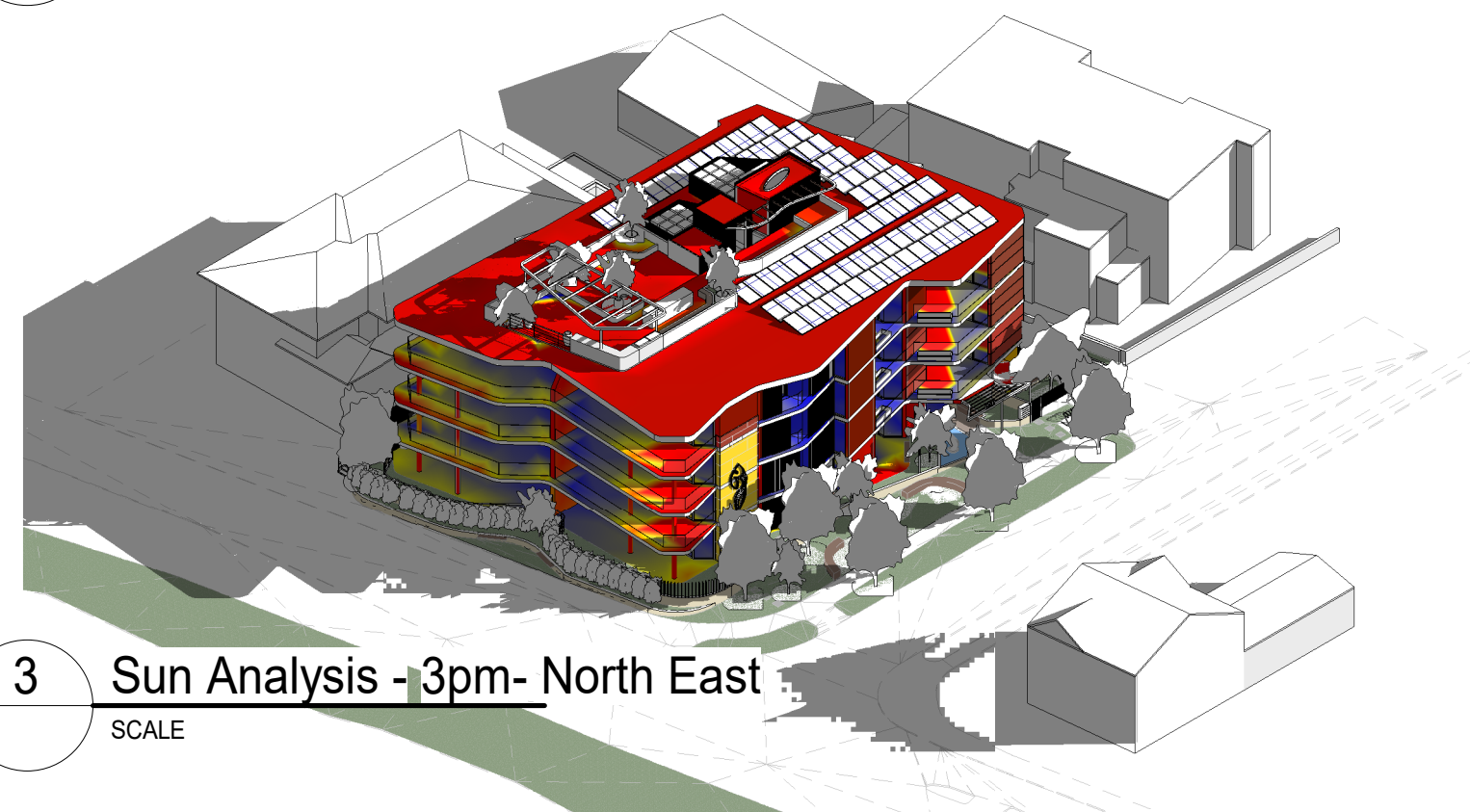
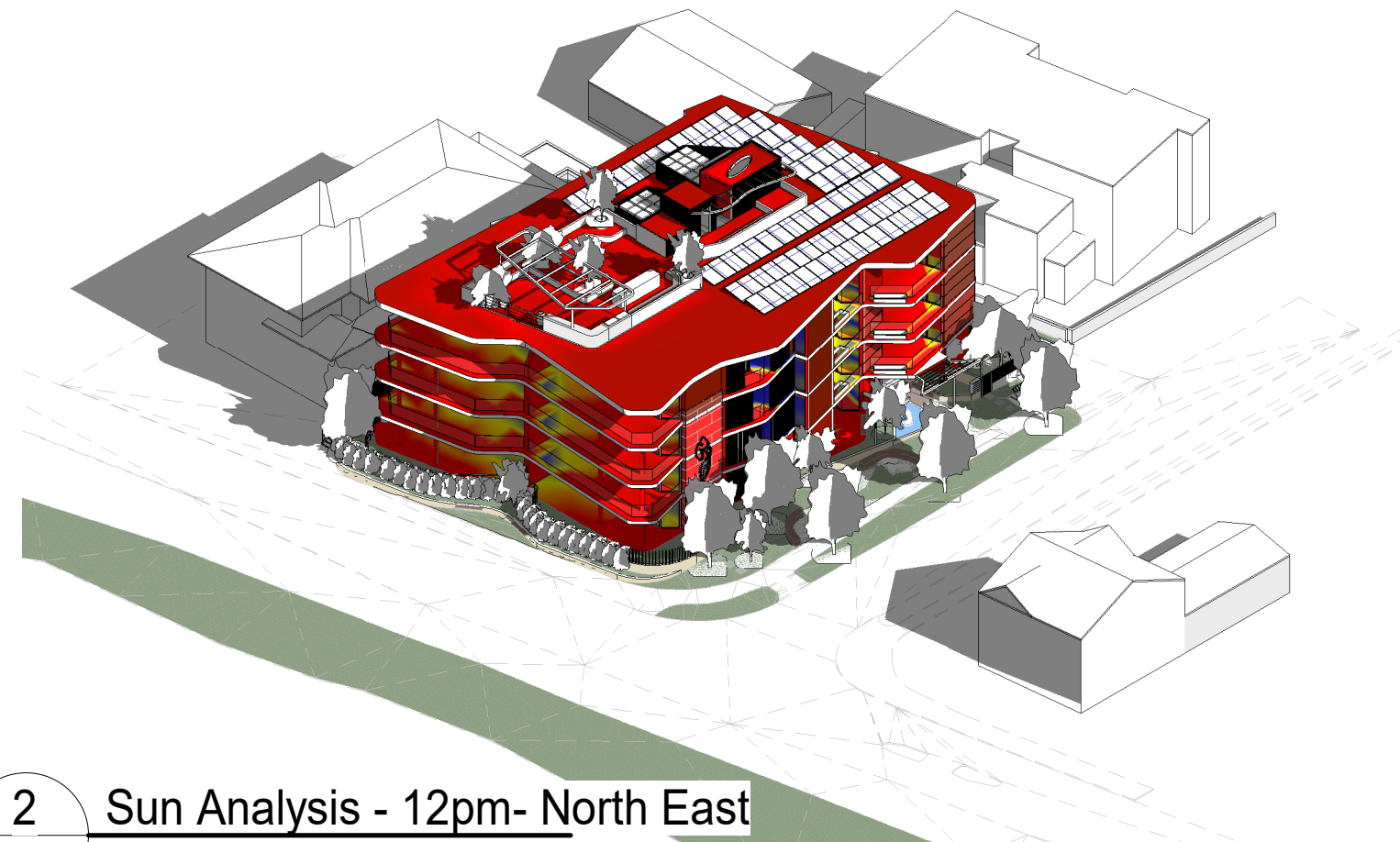
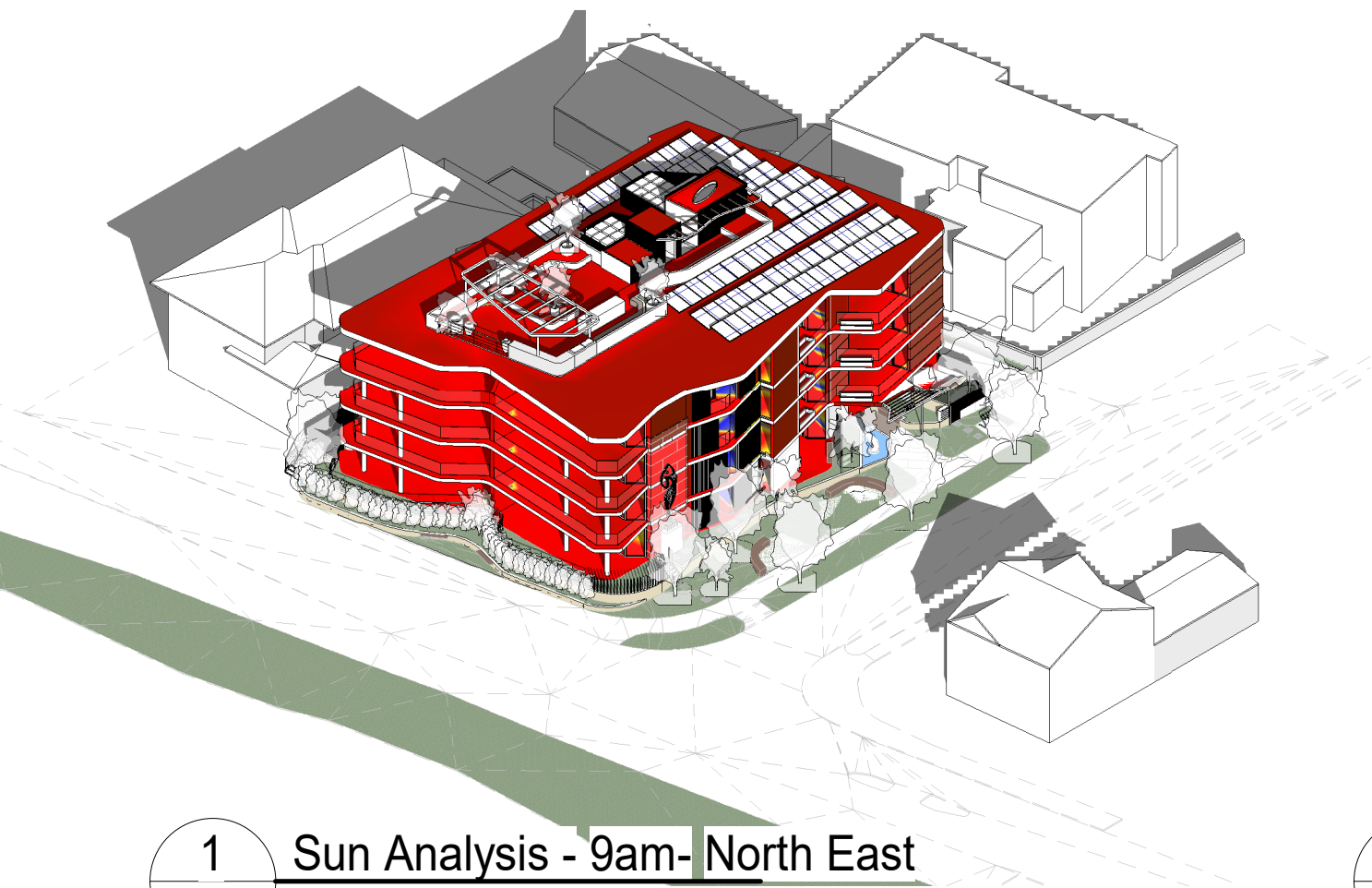
SUITE 1 - 57A BAY VIEW TERRACE CLAREMONT WA 6010
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A454- Proposed Shadow Diagram - October
25, The Esplanade, Mount Pleasant WA 6153
2010- Helm- Development Approval
@ A3



COLLIÈRE
ARCHITECTURE





1 Sun Analysis - 9am- North East

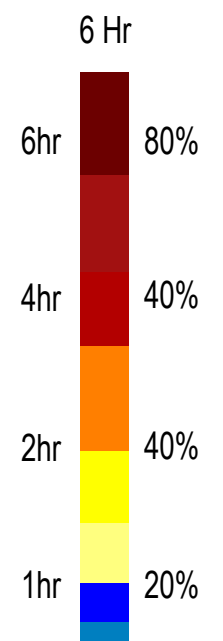
SCALE

2 Sun Analysis - 12pm- North East

SCALE

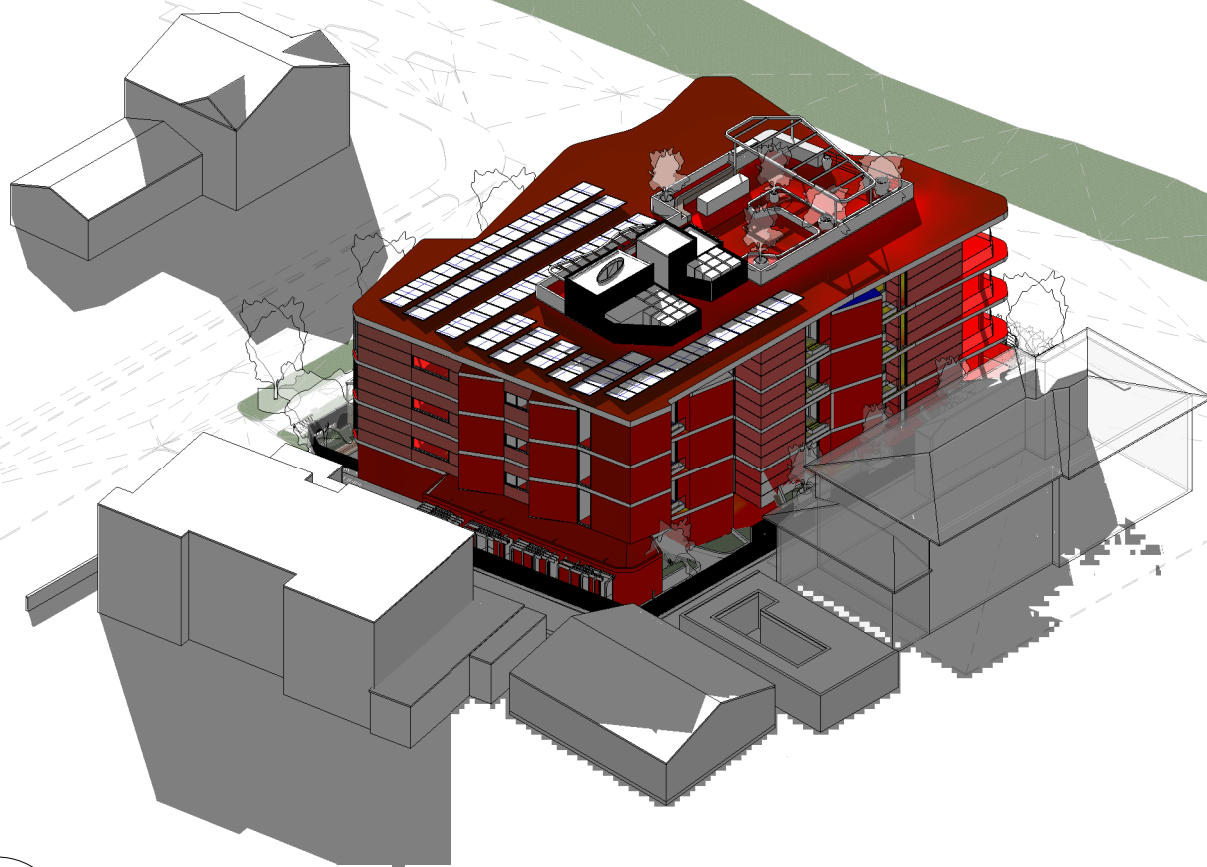
3 Sun Analysis - 3pm- North East

SCALE



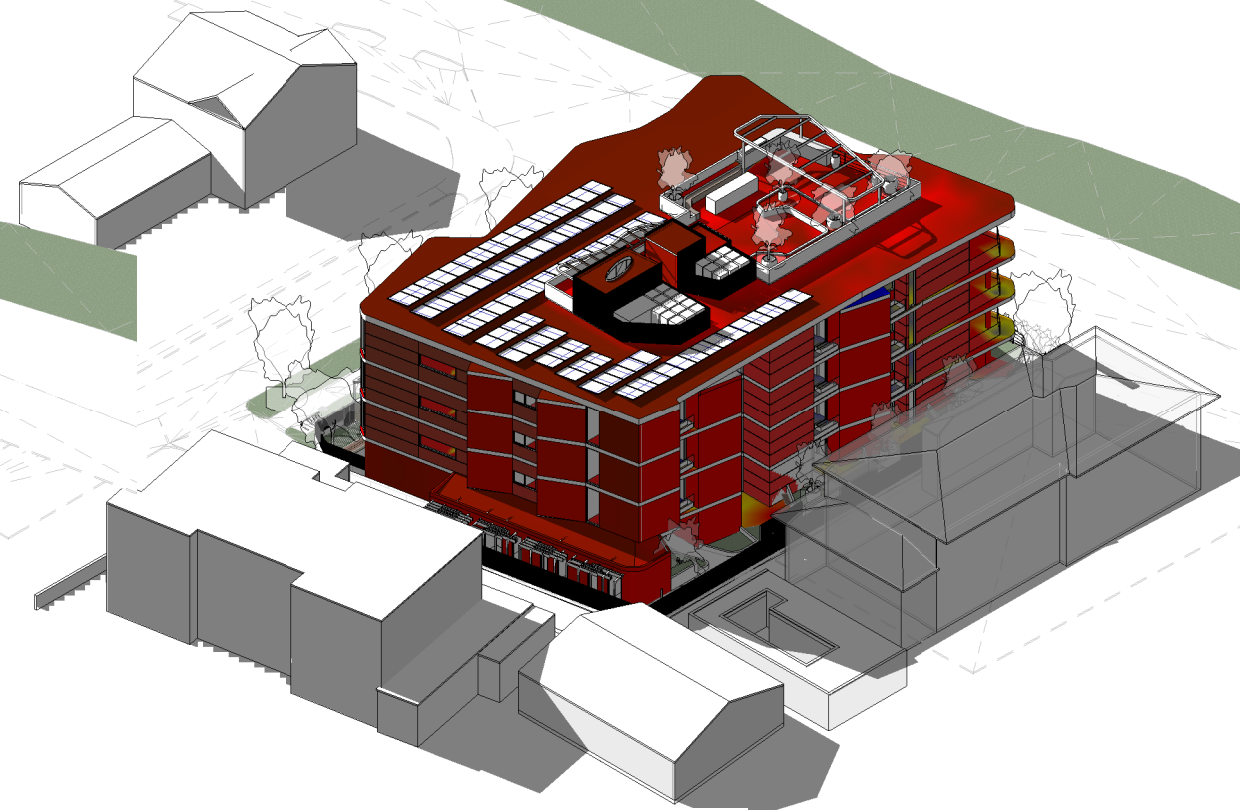
HOURS OF DIRECT
SUNLIGHT RECEIVED
BETWEEN 9AM & 3PM ON
21ST JUNE 2020

1A	4-6hr	2A	4-6hr	3A	4-6hr	4A	4-6hr
1B	4-6hr	2B	4-6hr	3B	4-6hr	4B	4-6hr
1C	4-6hr	2C	4-6hr	3C	4-6hr	4C	4-6hr



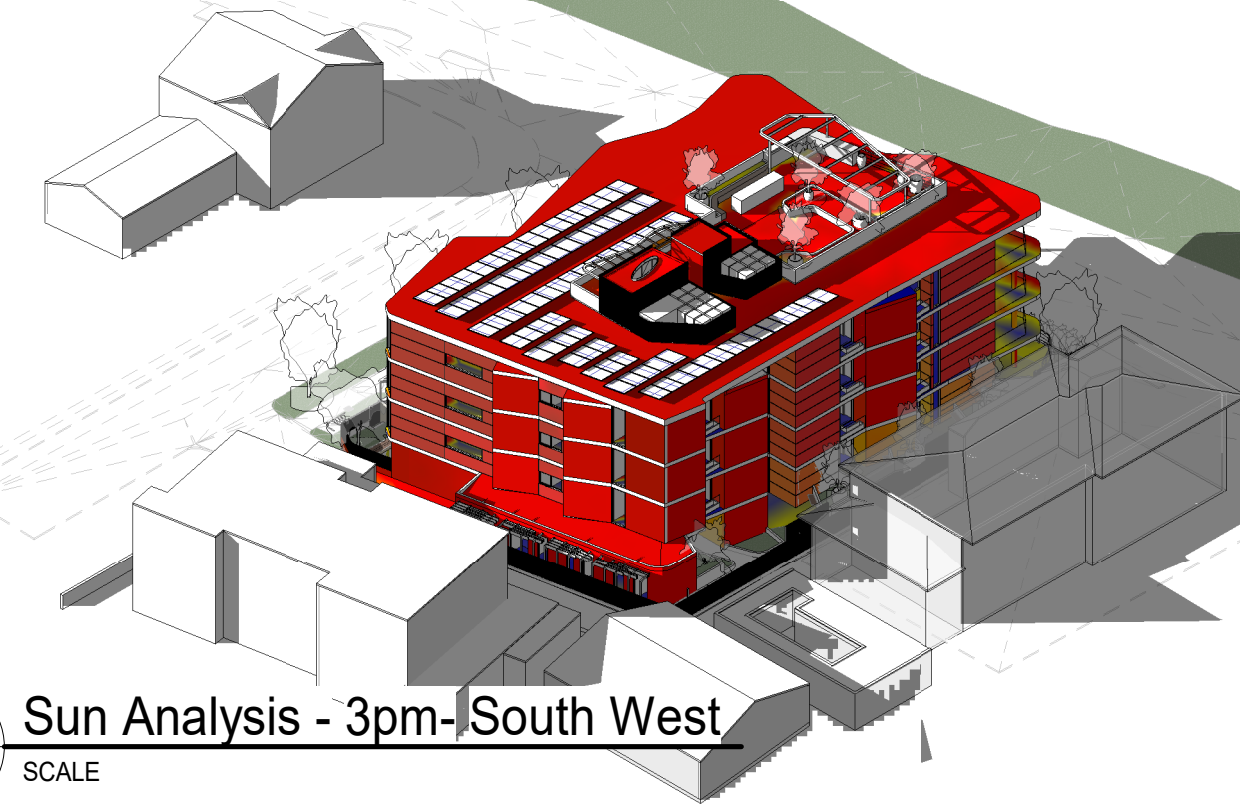
1 Sun Analysis - 9am- South West

SCALE



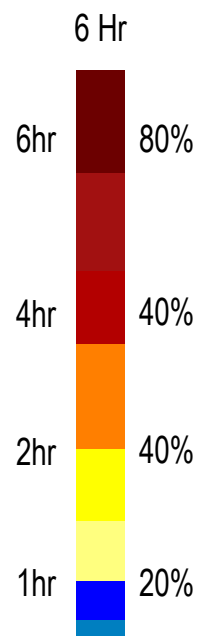
2 Sun Analysis - 12pm- South West

SCALE



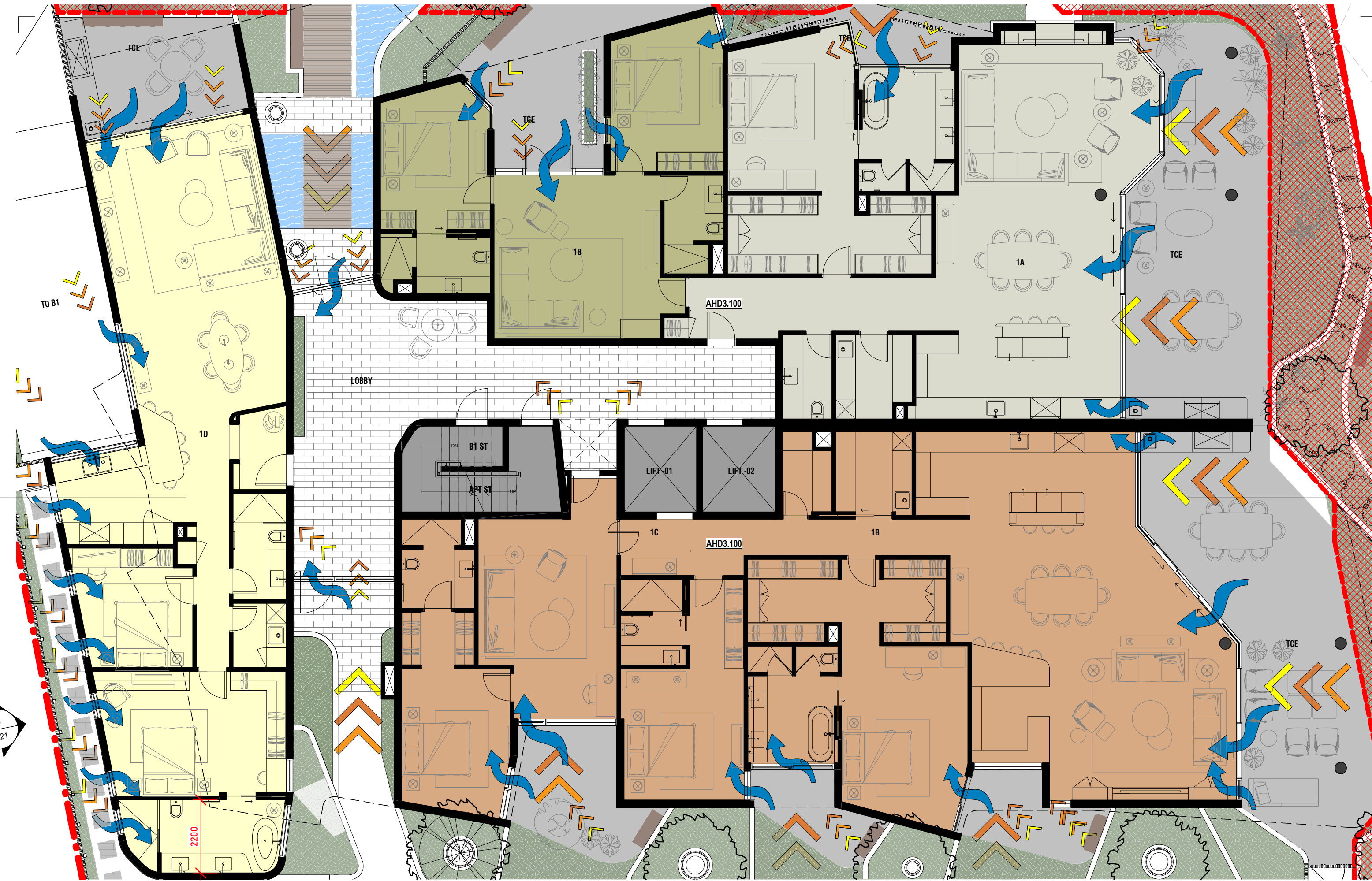
3 Sun Analysis - 3pm- South West

SCALE



HOURS OF DIRECT
SUNLIGHT RECEIVED
BETWEEN 9AM & 3PM ON
21ST JUNE 2020

1A	2-4hr	2A	5-6hr	3A	6hr	4A	6hr
1B	2-4hr	2B	6hr	3B	6hr	4B	6hr
1C	5-6hr	2C	6hr	3C	6hr	4C	6hr



ARCHITECTURE
 MASTER PLANNING
 URBAN DESIGN
 INTERIORS

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 E admin@colliere.com.au

DWG No. A470 - Cross Ventilation & Natural Light - GFL
 25, The Esplanade, Mount Pleasant WA 6153
 Project No. 2010- Helm - Development Approval
 1 : 100 @ A3

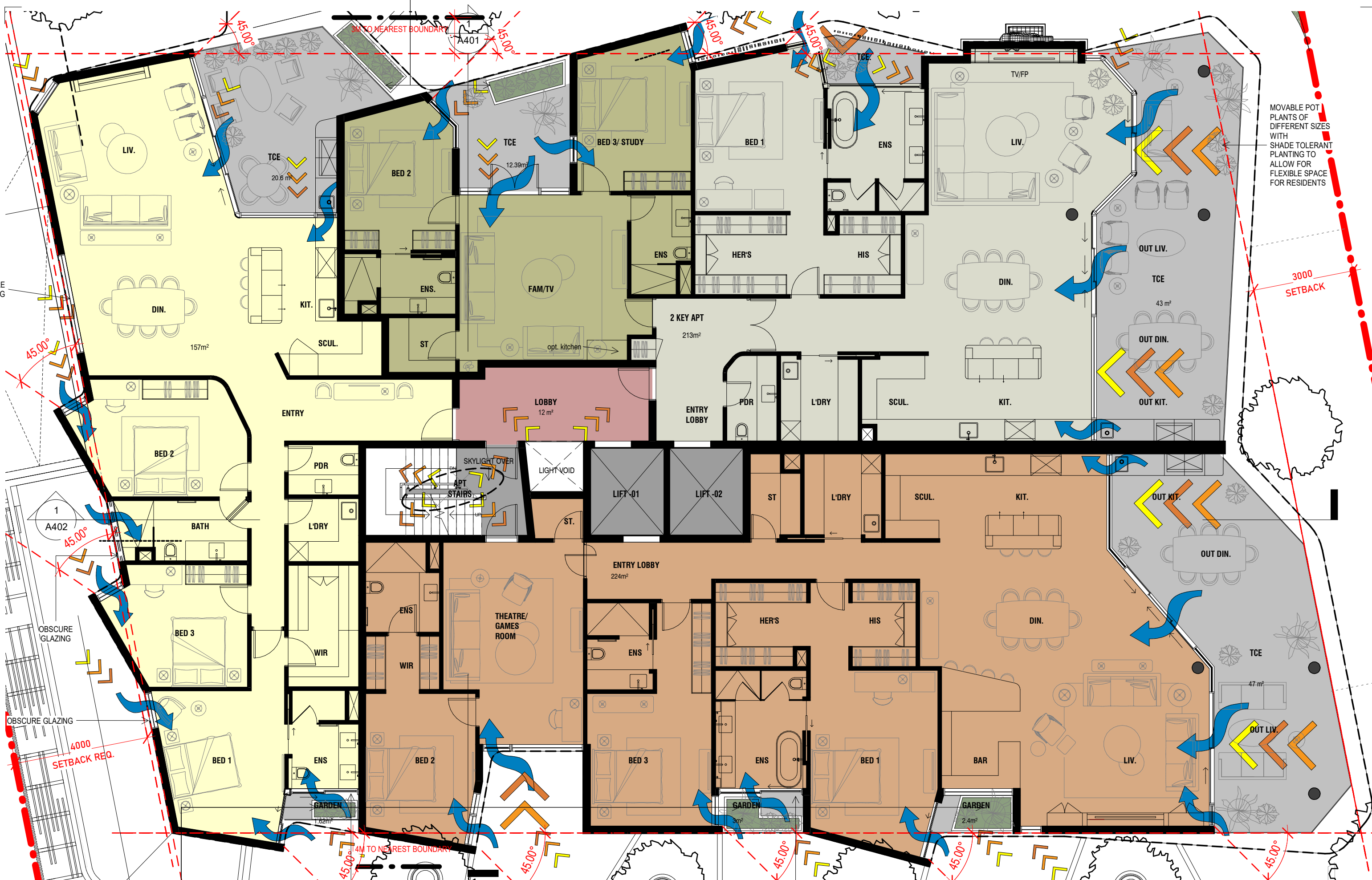
0 1 2 3 4 5
 SCALE METRES

GENERIS
UNIQUE SOLUTIONS FOR OWNER LED STRATA DEVELOPMENTS

PYRAMID
group australia

COLLIÈRE
ARCHITECTURE

25/05/2021 5:35:25 PM



ARCHITECTURE
 MASTER PLANNING
 URBAN DESIGN
 INTERIORS

SUITE 1 - 57A BAY VIEW TERRACE CLAREMONT WA 6010
 PO BOX 1010 CLAREMONT WESTERN AUSTRALIA 6010
 T (08) 9385 2858
 E admin@colliere.com.au

DWG No. A471 - Cross Ventilation & Natural Light - Typ. Apt
 25, The Esplanade, Mount Pleasant WA 6153
 Project No. 2010- Helm - Development Approval
 1 : 100 @ A3

SCALE METRES

GENERIS
UNIQUE SOLUTIONS FOR OWNER LED STRATA DEVELOPMENTS

PYRAMID
group australia

COLLIÈRE
ARCHITECTURE

25/05/2021 5:35:27 PM